

THE PORT AUTHORITY OF NY & NJ

**LAGUARDIA AIRPORT
REPLACEMENT OF THE
EAST FIELD LIGHTING VAULT**

**WORK ORDER No. X (TBD)
CONTRACT No. LGA-124.223**

30% SUBMISSION
07/15/2013

No.	Date	Revision	Approved

FEDERAL AVIATION ADMINISTRATION

ASSISTANT CHIEF ENGINEER/DESIGN

DATE

PROGRAM DIRECTOR AVIATION

DATE

CHIEF ENGINEER

DATE

Drawing
Number

G001

PID# 12801327

INDEX OF DRAWINGS

GENERAL

- G001 TITLE SHEET
- G002 INDEX OF DRAWINGS, GENERAL NOTES, LOCATION MAP AND CONSTRUCTION STAGING
- G003 LOCATION PLAN, ACCESS ROUTE, AND CONSTRUCTION CLEARANCES

CIVIL

- C001 CIVIL NOTES, LEGEND AND ABBREVIATIONS
- C002 REMOVAL PLAN
- C003 ALIGNMENT PLAN
- C004 PAVING PLAN
- C005 GRADING PLAN
- C006 PROFILE
- C007 UTILITY PLAN
- C008 DETAILS -1-
- C009 DETAILS -2-
- C010 DETAILS -3-
- C011 DETAILS -4-

ARCHITECTURAL

- A001 EAST FIELD LIGHTING VAULT PROPOSED FLOOR PLAN
- A002 EAST FIELD LIGHTING VAULT PROPOSED ROOF PLAN
- A003 EAST FIELD LIGHTING VAULT PROPOSED ELEVATIONS #1
- A004 EAST FIELD LIGHTING VAULT PROPOSED ELEVATIONS #2

STRUCTURAL

- S001 STRUCTURAL NOTES, INDEX AND ABBREVIATIONS
- S002 FOUNDATION PLAN
- S003 GROUND FLOOR PLAN
- S004 ROOF FRAMING PLAN
- S005 NORTH ELEVATION
- S006 SOUTH ELEVATION

LANDSCAPING

- LS001

ELECTRICAL

- E100 EXISTING EFLV ONE LINE DIAGRAM
- E101 EFLV ONE LINE DIAGRAM

- E200 REMOVAL PLAN -1-
- E201 REMOVAL PLAN -2-
- E202 REMOVAL PLAN -3-
- E203 REMOVAL PLAN -4-
- E204 REMOVAL PLAN -5-
- E205 REMOVAL PLAN -6-
- E206 REMOVAL PLAN -7-
- E207 REMOVAL PLAN -8-
- E208 REMOVAL PLAN -9-
- E209 REMOVAL PLAN -10-
- E210 REMOVAL PLAN -11-
- E211 PARTIAL REMOVAL PLAN -1-
- E212 INSTALLATION PLAN -1-
- E213 INSTALLATION PLAN -2-
- E214 INSTALLATION PLAN -3-
- E215 INSTALLATION PLAN -4-
- E216 INSTALLATION PLAN -5-
- E217 INSTALLATION PLAN -6-
- E218 INSTALLATION PLAN -7-
- E219 INSTALLATION PLAN -8-
- E220 INSTALLATION PLAN -9-
- E221 INSTALLATION PLAN -10-
- E222 INSTALLATION PLAN -11-
- E223 PARTIAL PLAN -1-

- E251 MAIN ELECTRICAL EQUIPMENT INSTALLATION PLAN

MECHANICAL

- M001

FIRE PROTECTION

- FP001

PLUMBING

- PO01

ELECTRONICS SYSTEMS

- ES201 FIELD LIGHTING VAULT - ELECTRONICS FLOOR PLAN
- ES301 FIELD LIGHTING VAULT - ALCS EQUIPMENT ROOM
- ES501 FIBER OPTIC BLOCK DIAGRAM

GENERAL NOTES

1. VERIFY ALL DIMENSIONS AND CONDITIONS SHOWN ON THE CONTRACT DRAWINGS AT THE SITE BEFORE ORDERING ANY MATERIAL, EQUIPMENT OR PERFORMING ANY WORK.
2. ALL AREAS WHICH ARE DAMAGED BY THE CONTRACTOR SHALL BE RESTORED OR REPLACED BY HIM TO THEIR ORIGINAL CONDITION TO THE SATISFACTION OF THE ENGINEER AT NO ADDITIONAL COST TO THE AUTHORITY.
3. PROVIDE WRITTEN NOTICE TO THE ENGINEER, 5 DAYS IN ADVANCE, FOR ANY SHUTDOWN/STARTUP REQUIREMENTS. COORDINATE SHUTDOWNS/STARTUPS TO MINIMIZE IMPACTS TO THE CONTINUOUS OPERATION OF THE FACILITY.
4. THE CONTRACTOR SHALL GIVE THE ENGINEER 5 DAYS ADVANCE NOTICE OF HIS INTENTION TO START FIELD WORK. FIELD WORK SHALL NOT BE STARTED UNTIL ALL NECESSARY EQUIPMENT AND MATERIALS ARE AT THE CONSTRUCTION SITE.
5. PRIOR TO STARTING ANY REMOVAL WORK, MAKE AN INSPECTION WITH THE ENGINEER TO DETERMINE FIELD CONDITIONS.
6. ALL MATERIAL AND EQUIPMENT TO BE REMOVED SHALL BE DISPOSED OFF AUTHORITY PROPERTY BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE AUTHORITY.
7. WORK AREAS SHALL BE CLEARED OF ALL CONSTRUCTION DEBRIS AT THE COMPLETION OF CONTRACT WORK.
8. OBTAIN PORT AUTHORITY CUTTING AND WELDING PERMIT FROM THE ENGINEER. PROVIDE MECHANICAL VENTILATION OF SUFFICIENT CAPACITY IN THE WORK AREA HAVING WELDING, BRAZING AND CUTTING ACTIVITIES IN ACCORDANCE WITH OSHA'S CONSTRUCTION STANDARDS.
9. PREVENT WATER PONDING RESULTING FROM CONSTRUCTION OPERATIONS. PROMPTLY REMOVE ANY PONDED WATER TO THE SATISFACTION OF THE ENGINEER, AT NO ADDITIONAL COST TO THE AUTHORITY.
10. IMMEDIATELY RESTORE, TO ITS ORIGINAL CONDITION, ALL AREAS OUTSIDE THE LIMITS OF CONSTRUCTION, WHICH ARE DAMAGED BY THE CONTRACTOR'S OPERATIONS, TO THE SATISFACTION OF THE ENGINEER AT NO ADDITIONAL COST TO THE AUTHORITY.
11. ALL REGRADED AREAS AND SOIL DISTURBED BY THE CONTRACTOR AS A RESULT OF CONSTRUCTION OPERATIONS SHALL BE TESTED AS PER SPECIFICATION SECTION 02920 "SOIL TESTING".
12. PERFORM ALL SEEDING OPERATIONS IN ACCORDANCE WITH SPECIFICATION SECTION 02930 "SEEDING". ADD COMPOST IN ACCORDANCE WITH SPECIFICATION SECTION 02960 "ADDING COMPOST".
13. UTILIZE A MOBILE SWEEPER AND WATER TRUCK TO CONTROL DUST AND DEBRIS ALONG THE HAUL ROUTE AND AREA OF WORK AT THE END OF EACH WORK SHIFT.

CONSTRUCTION STAGING

GENERAL:

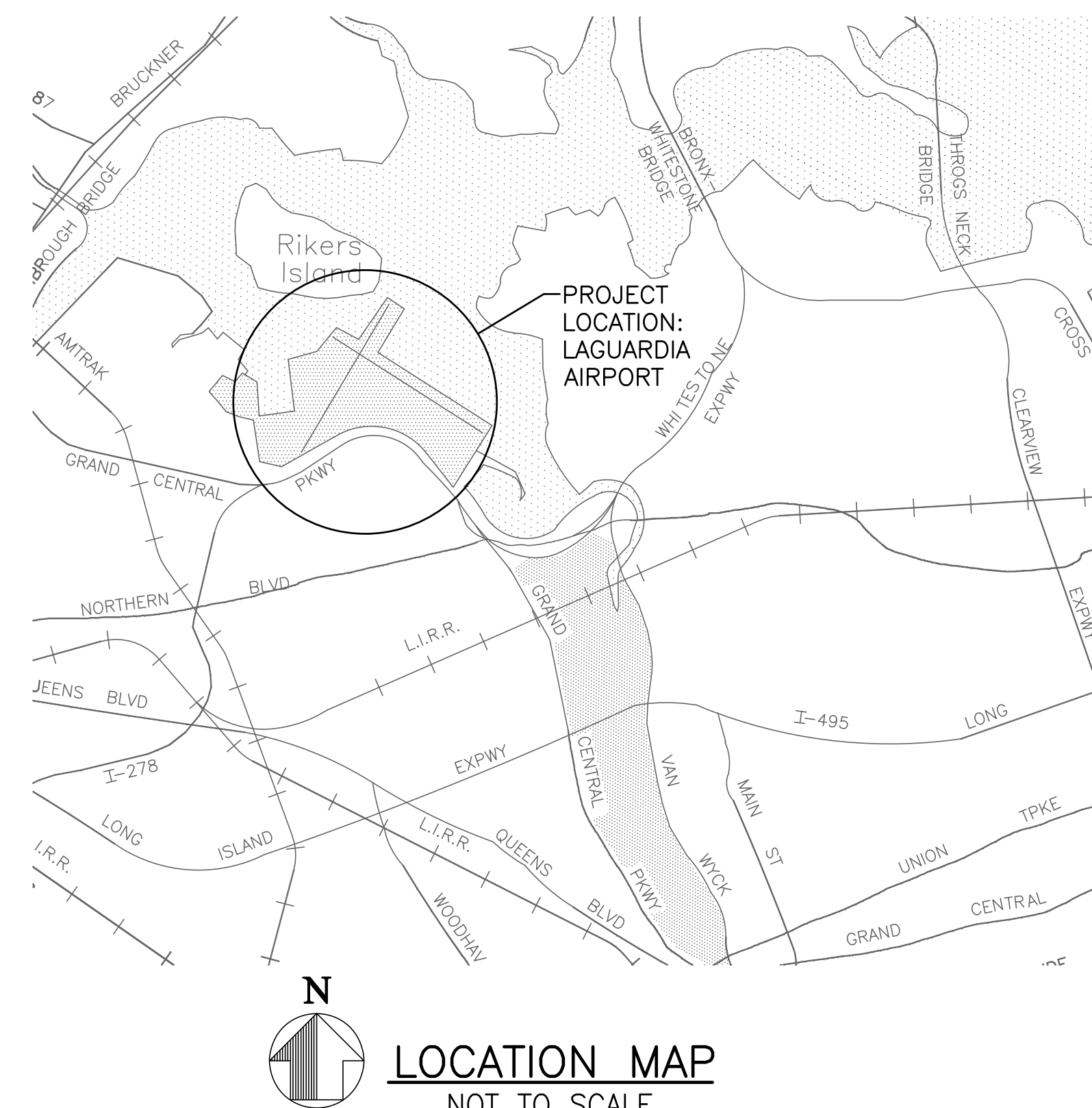
1. THE EXISTING EAST FIELD LIGHTING VAULT SHALL REMAIN OPERATIONAL IN IT'S ENTIRETY UNTIL A POINT IN WHICH THE REPLACEMENT EAST FIELD LIGHTING VAULT IS FULLY OPERATIONAL AND READY TO ACCEPT THE TRANSITION OF AIRFIELD LIGHTING LOADS.
2. COORDINATE WITH THE ENGINEER FOR ALL SHUTDOWNS OF EXISTING EQUIPMENT FOR REMOVALS AND INSTALLATIONS A MINIMUM OF 5 DAYS PRIOR TO BEGINNING THE WORK.
3. CONTRACTOR SHALL SUBMIT STAGING PLAN TO ENGINEER FOR APPROVAL.
4. ALL WORK SHALL BE COORDINATED WITH THE ENGINEERING AND FACILITY OPERATIONS FOR EQUIPMENT DELIVERIES TO THE WORK SITE.

NET COST NOTES

WHEN AND AS DIRECTED BY THE ENGINEER, THE CONTRACTOR SHALL PERFORM THE FOLLOWING WORK AND WILL BE COMPENSATED AT THE NET COST THEREOF:

1. CONSTRUCTION STAGING, SCHEDULE CHANGES TO ACCOMMODATE FACILITY OPERATIONAL REQUIREMENTS.
2. REPLACEMENT OF DAMAGED SHEETMETAL NOT SHOWN ON THE CONTRACT DRAWINGS.
3. RELOCATION OF UNDERGROUND UTILITIES NOT SHOWN ON THE CONTRACT DRAWINGS.
4. REPAIR OF DAMAGED UNDERGROUND UTILITIES NOT SHOWN ON CONTRACT DRAWINGS.
5. TRANSPORTATION AND DISPOSAL OF EXCESS/UNSUITABLE EXCAVATED MATERIAL, COLLECTION AND DISPOSAL OF PIT DEBRIS AND DISPOSAL OF SANDBLASTED SHEETING MATERIAL.
6. CLEAN BACKFILL MATERIAL WHERE SUBGRADE MAY BE UNSUITABLE OR SATURATED.
7. MISCELLANEOUS FENCE AND RAZOR RIBBON REPAIRS NOT SHOWN ON THE CONTRACT DRAWINGS.

THE NET COST SHALL BE COMPUTED IN THE SAME MANNER AS IS COMPENSATION FOR EXTRA WORK, INCLUDING ANY PERCENTAGE ADDITION TO COST, AS SET FORTH IN THE CLAUSE OF THE CONTRACT ENTITLED COMPENSATION FOR "EXTRA WORK". PERFORMANCE OF SUCH NET COST WORK SHALL BE AS DIRECTED BY THE ENGINEER AND SHALL BE SUBJECT TO ALL THE PROVISIONS OF THE CONTRACT RELATING TO THE PERFORMANCE OF EXTRA WORK. COMPENSATION FOR SAID NET COST WORK SHALL NOT BE CHARGED AGAINST THE TOTAL AMOUNT OF COMPENSATION AUTHORIZED FOR EXTRA WORK.



CHIEF ELECTRICAL ENGINEER

30% SUBMISSION
07/15/2013

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ENGINEERING DEPARTMENT

LAGUARDIA AIRPORT

GENERAL

Title
REPLACEMENT OF THE EAST FIELD LIGHTING VAULT

INDEX OF DRAWINGS, GENERAL NOTES, LOCATION MAP AND CONSTRUCTION STAGING

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G. GADOMSKI **G. GADOMSKI** **E. FAYFMAN**
Designed by Drawn by Checked by

Date **07/15/2013**

WORK ORDER No. # X (TBD)
Contract Number **LGA - 124.223**

Drawing Number **G002**
PID# 12801327

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**LAGUARDIA
AIRPORT**

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**LOCATION PLAN,
ACCESS ROUTE, AND
CONSTRUCTION
CLEARANCES**

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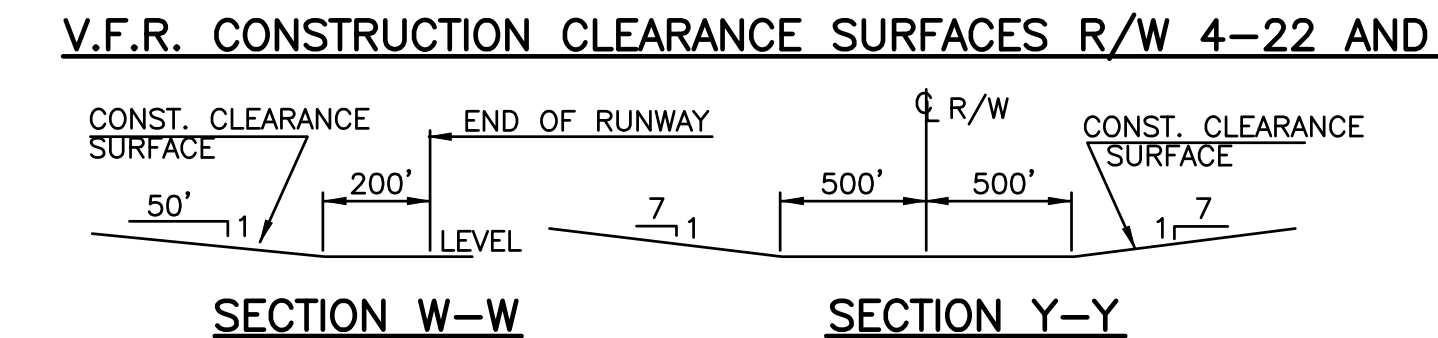
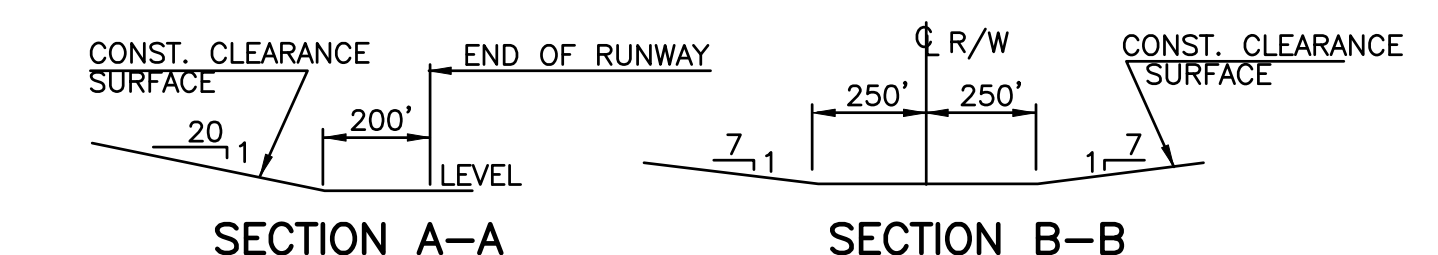
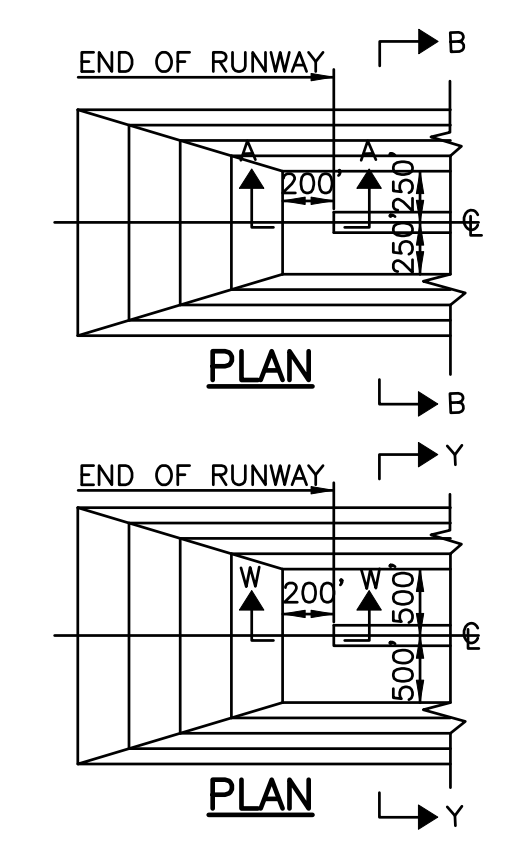
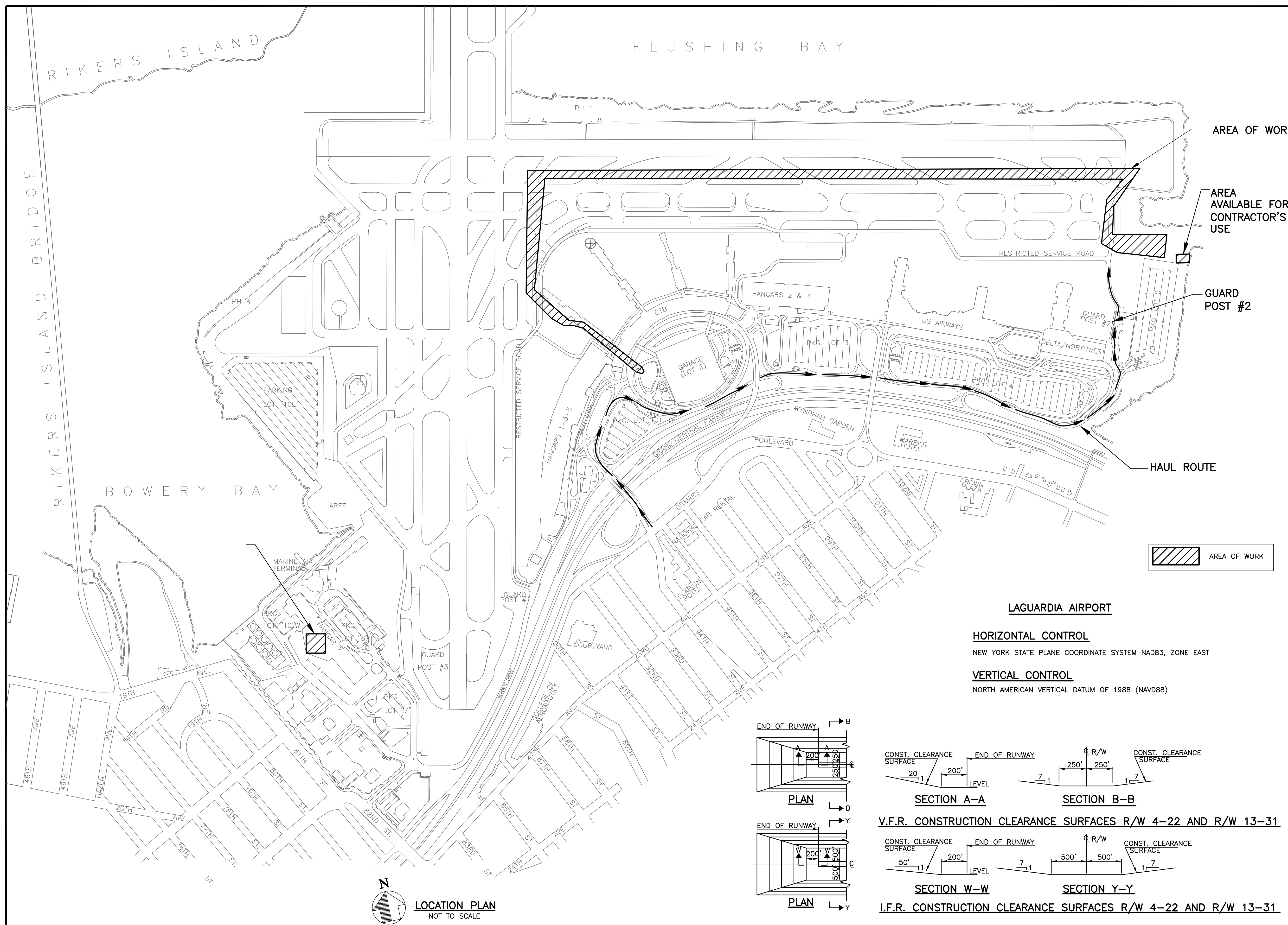
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I.F.R. CONSTRUCTION CLEARANCE SURFACES R/W 4-22 AND R/W 13-31

LOCATION PLAN
NOT TO SCALE

LEGEND (FOR DWG. NO. C002 TO C007 ONLY)

EXISTING	REMOVE	NEW	
			CONCRETE CURB
			SAWCUT, MEET EXISTING PAVEMENT AND GRADE
			DRAINAGE LINE
			CONSTRUCTION FENCE
			CONSTRUCTION BARRIER WITH FENCE
			AIRSIDE PAVEMENT MARKINGS
			CLEAN OUT
			ELECTRICAL BOX
			CATCH BASIN
			MANHOLE
			SIGN
			SPOT ELEVATION
			CONTOUR

CONCRETE SIDEWALK/ISLAND
(4" PCC, 4" DGABC)

ASPHALT CONCRETE PAVEMENT - ROADWAY
(3" AC, 4" PMM, 6" DGABC)

ASPHALT CONCRETE PAVEMENT - APRON
(8" AC, 7" PMM, 9" DGABC)

REMOVE EXISTING GRAVEL (12"±) BELOW FINISHED GRADE

TURF GRASS SWALE

REMOVAL	NEW

CIVIL NOTES:

- CHECK THAT ALL MANHOLE COVERS, CATCH BASIN GRATES AND OTHER CASTINGS WITHIN THE WORK AREA ARE SECURED BEFORE OPENING ROADWAY TO TRAFFIC.
- THE LOCATION, ELEVATION AND SIZE OF EXISTING UTILITIES SHOWN ON THE CONTRACT DRAWINGS ARE APPROXIMATE. THE FOLLOWING SHALL BE PERFORMED:
 - VERIFY THE LOCATION OF EXISTING UNDERGROUND UTILITIES WITHIN THE WORK AREA PRIOR TO CONSTRUCTION.
 - EXERCISE EXTREME CAUTION WHEN WORKING ADJACENT TO EXISTING POWER, COMMUNICATIONS, WATER OR GAS LINES TO PREVENT DAMAGE OF THESE LINES.
 - IMMEDIATELY REPAIR ANY DAMAGE TO EXISTING UTILITIES CAUSED BY THE CONTRACTOR'S OPERATIONS IN A MANNER APPROVED BY THE ENGINEER, AT NO ADDITIONAL COST TO THE AUTHORITY.
- WHERE A DRAINAGE STRUCTURE IS TO BE CONSTRUCTED ON EXISTING PIPE, VERIFY THE LOCATIONS AND INVERTS OF THE EXISTING PIPE PRIOR TO CONSTRUCTION OR FABRICATION.
- WHERE CONNECTION TO EXISTING STRUCTURES ARE REQUIRED, CUT NEATLY WITHOUT PERCUSSION INTO THE EXISTING STRUCTURE. THE MAXIMUM SIZE OF THE OPENING SHALL NOT EXCEED THE PIPE'S OUTER DIAMETER PLUS 3 INCHES. CONNECT THE NEW PIPE AND SEAL AROUND IT WITH CEMENT MORTAR.
- CLEAN THE INTERIOR OF ALL EXISTING CATCH BASINS WITHIN THE AREA OF WORK. THE AVERAGE DEPTH OF CATCH BASIN IS 5 FEET.
- COLLECT SEDIMENT FROM CATCH BASINS LOCATED WITHIN THE AREA OF WORK AND PLACE IN DOT APPROVED CONTAINERS.
- TEST SEDIMENT FROM CATCH BASINS FOR DISPOSAL IN ACCORDANCE WITH THE RESOURCE CONSERVATION AND RECOVERY ACT (RCRA) AND THE REQUIREMENTS OF THE DISPOSAL FACILITY RECEIVING THE WASTE. SEE NET COST NOTES.

ABBREVIATIONS:

AC	ASPHALT CONCRETE	LP	LOW PRESSURE WATER
BC	BOTTOM OF CURB	MH	MANHOLE
CB	CATCH BASIN	N	NORTH
DGABC	DENSE GRADED AGGREGATE BASE COURSE	NTS	NOT TO SCALE
DIP	DUCTILE IRON PIPE	PCC	PORTLAND CEMENT CONCRETE
DWG	CONTRACT DRAWING	PMM	PLANT MIX MACADAM
E	ELECTRICAL OR EAST	RCP	REINFORCED CONCRETE PIPE
ELEV	ELEVATION	RIM	RIM ELEVATION
HP	HIGH PRESSURE WATER	S	SOUTH OR SANITARY SEWER
HYD	HYDRANT	TC	TOP OF CURB
INV	INVERT ELEVATION	TYP	TYPICAL
		W	WATER OR WEST
		WV	WATER VALVE

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**THE PORT AUTHORITY
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CHIEF CIVIL ENGINEER

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ENGINEERING DEPARTMENT

LAGUARDIA
AIRPORT

CIVIL

Title
REPLACEMENT OF THE EAST FIELD
LIGHTING VAULT

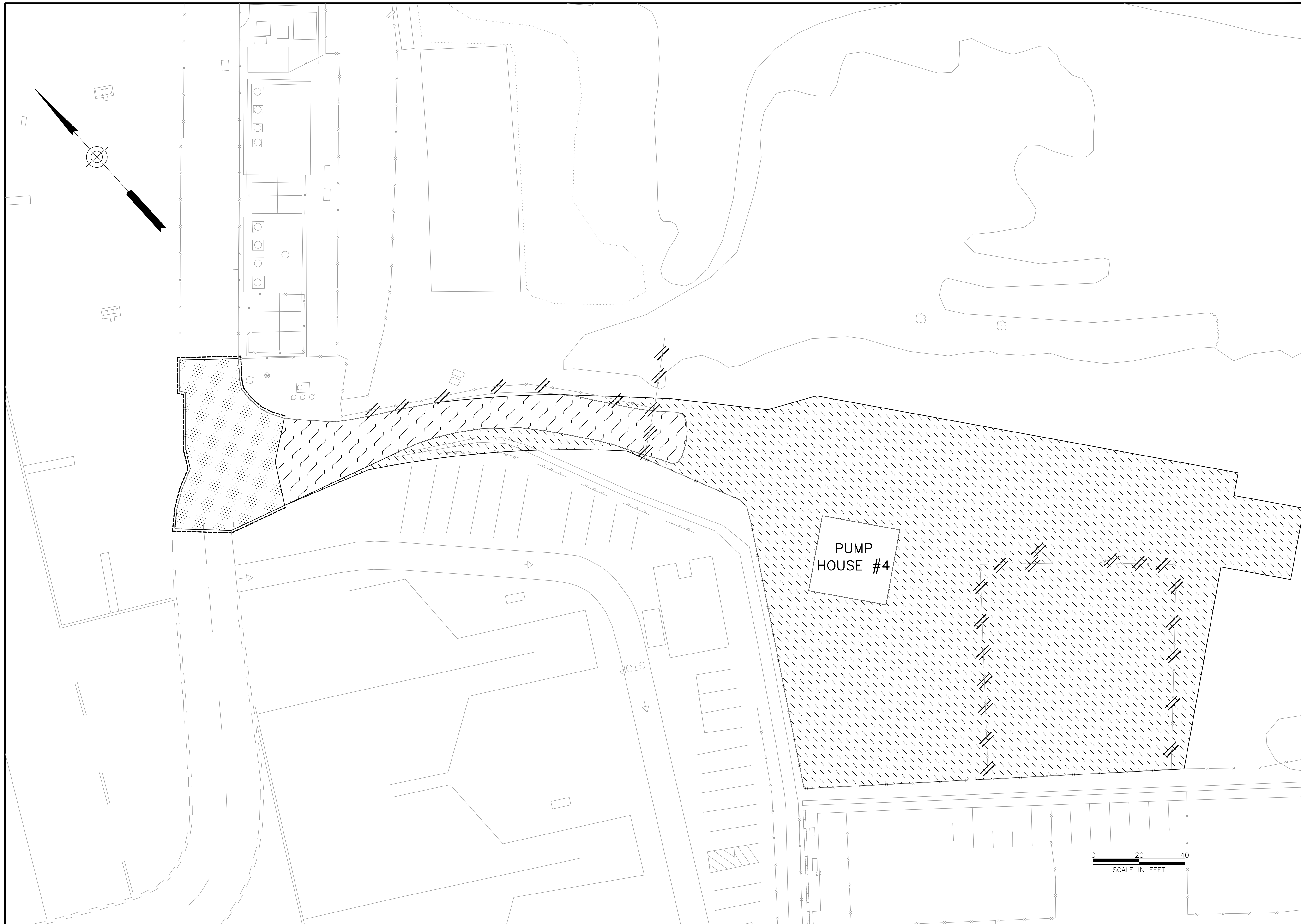
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A.WASTI/L.POLYANSKIY A.WASTI R.MALEKI
Designed by Drawn by Checked by

Date 07/15/2013
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Drawing
Number **C001**
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REMOVAL PLAN

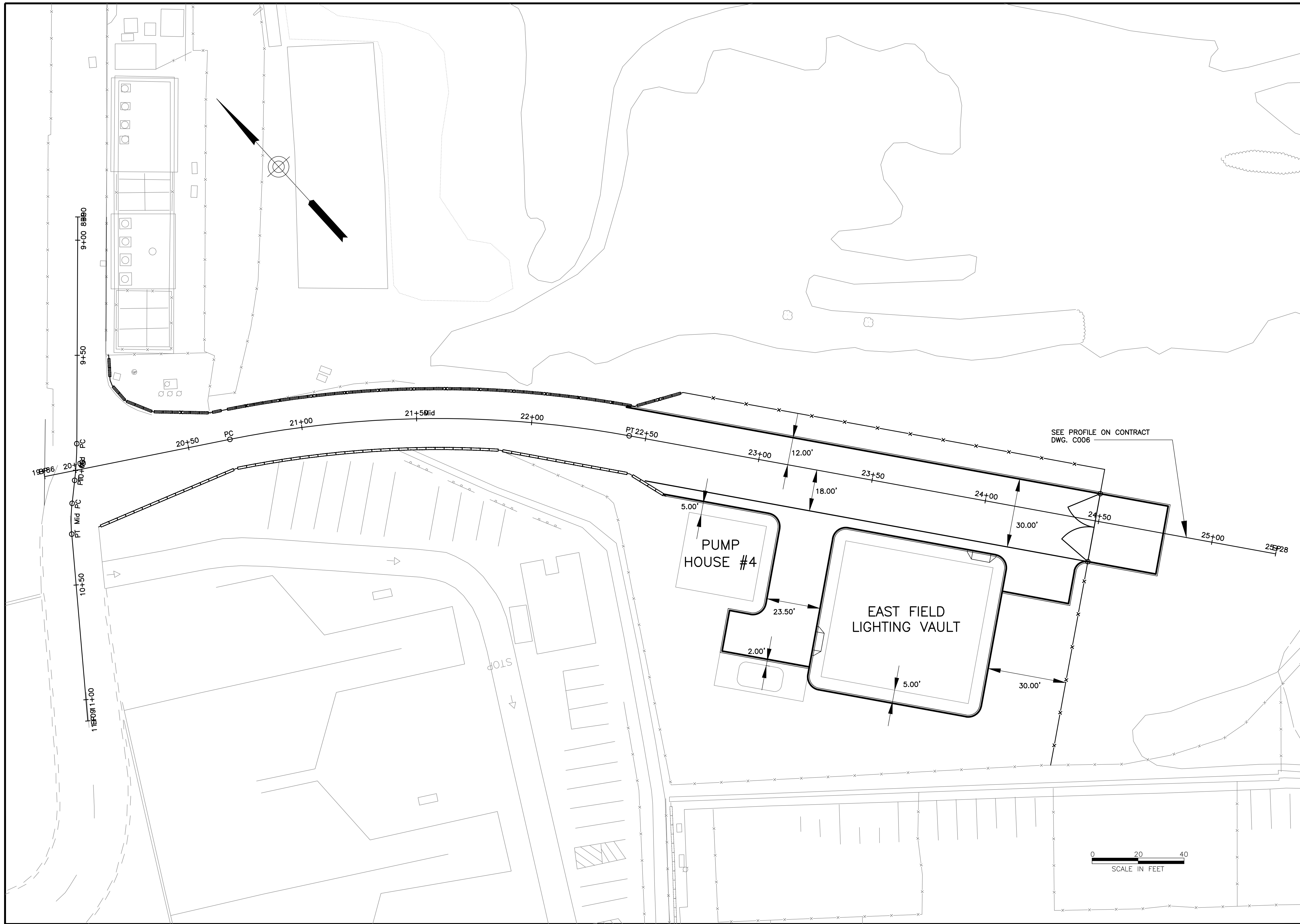
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I.POLYANSKIY/A.WASTI A.WASTI R.MALEKI
Designed by Drawn by Checked by

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LAGUARDIA AIRPORT			

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ALIGNMENT PLAN

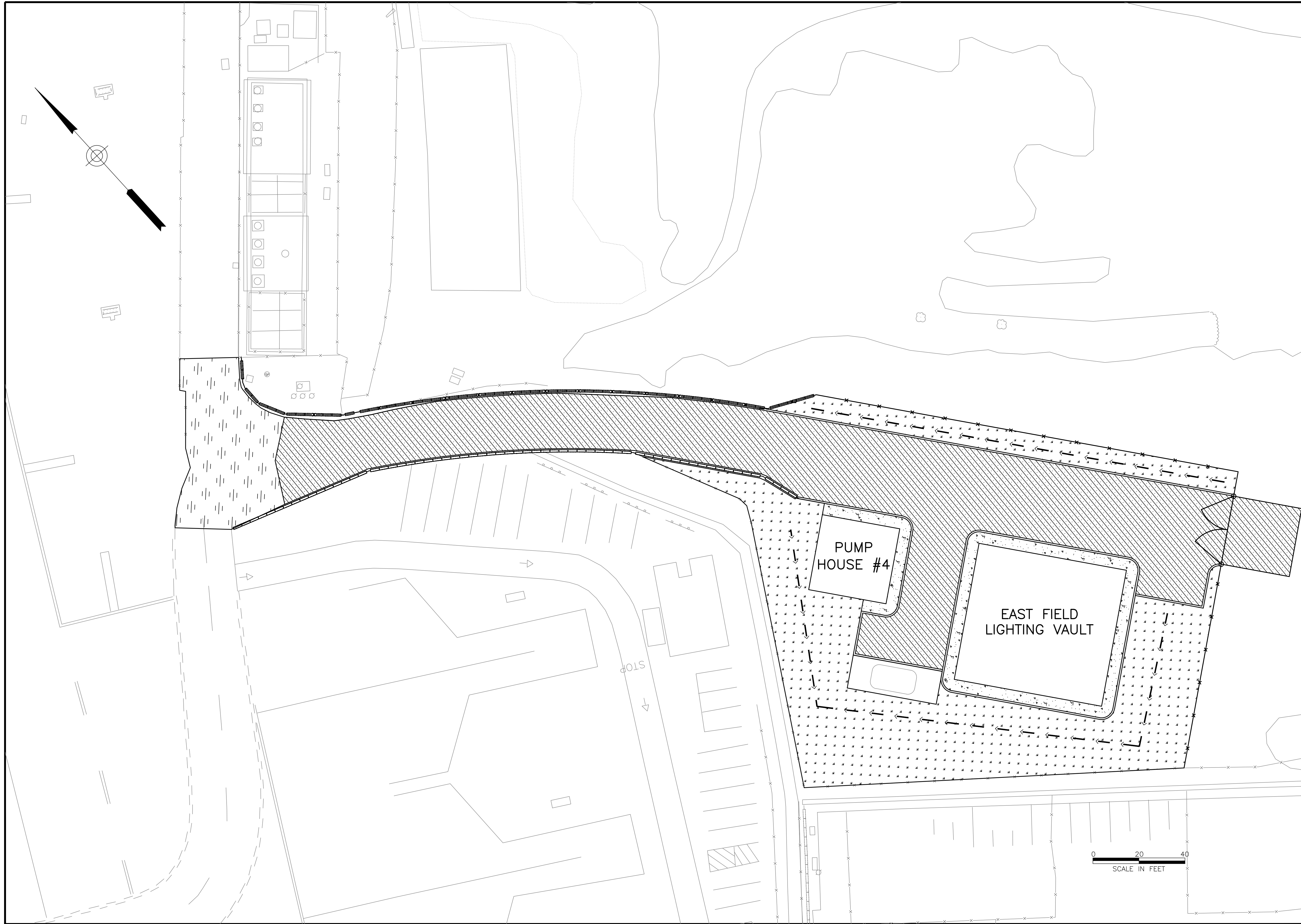
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Designed by **I.POLYANSKIY/A.WASTI** Drawn by **A.WASTI** Checked by **R.MALEKI**

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PAVING PLAN

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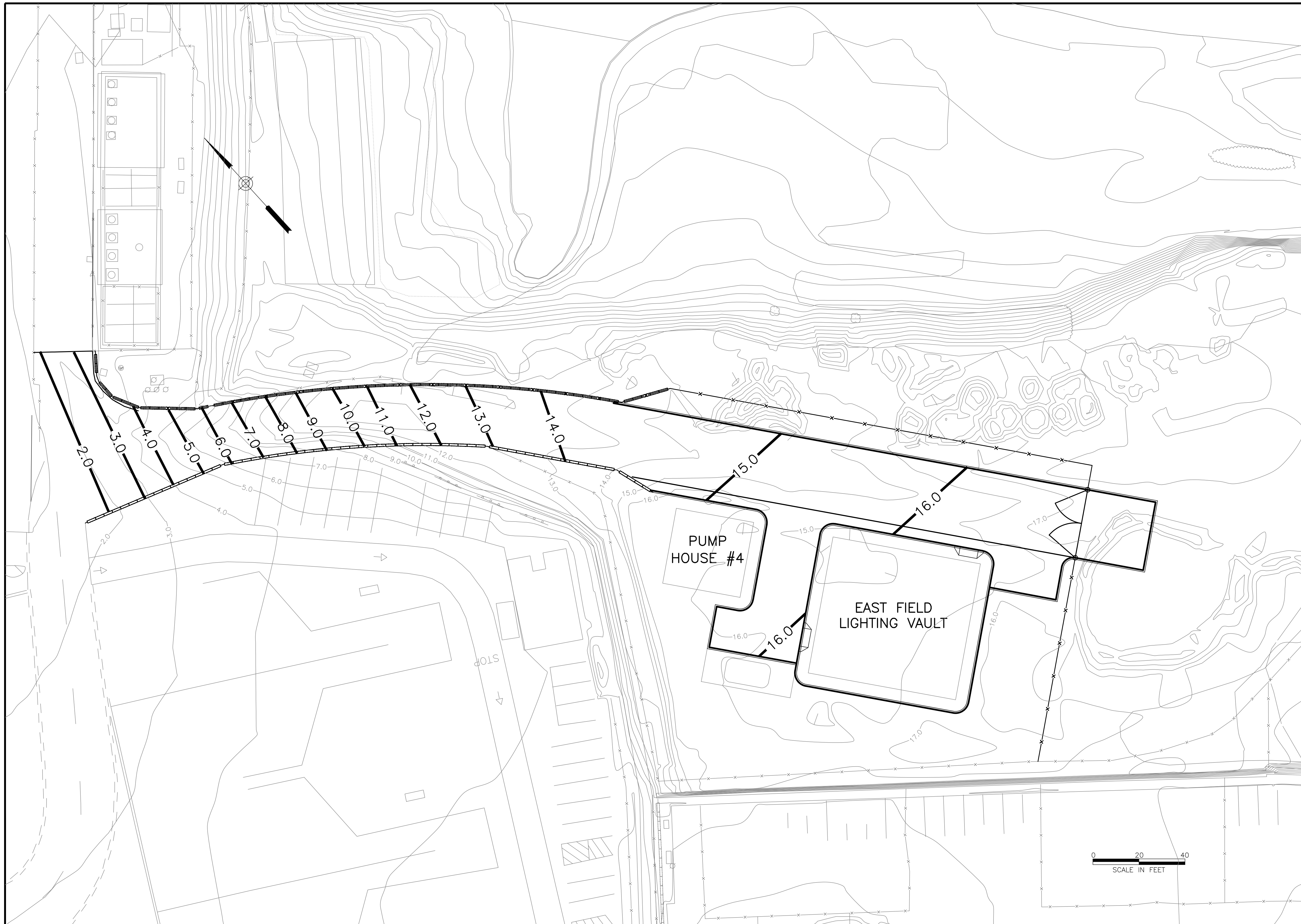
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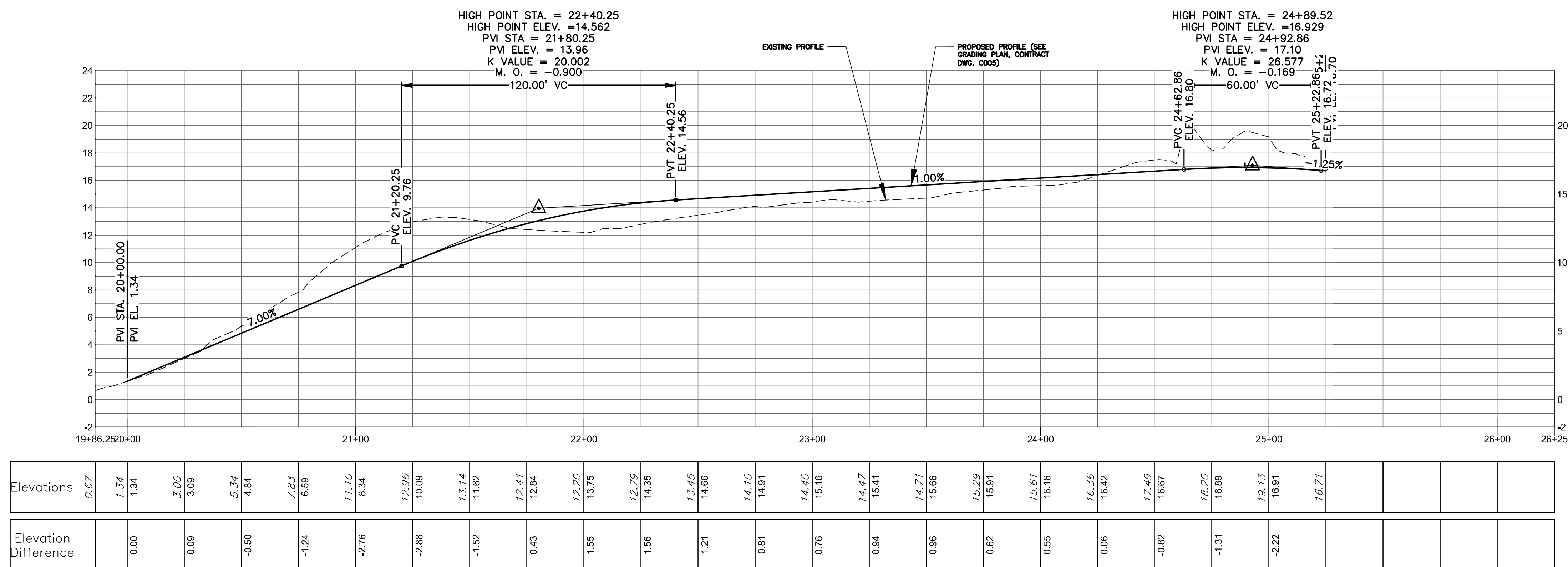
ENGINEERING DEPARTMENT

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PROFILE



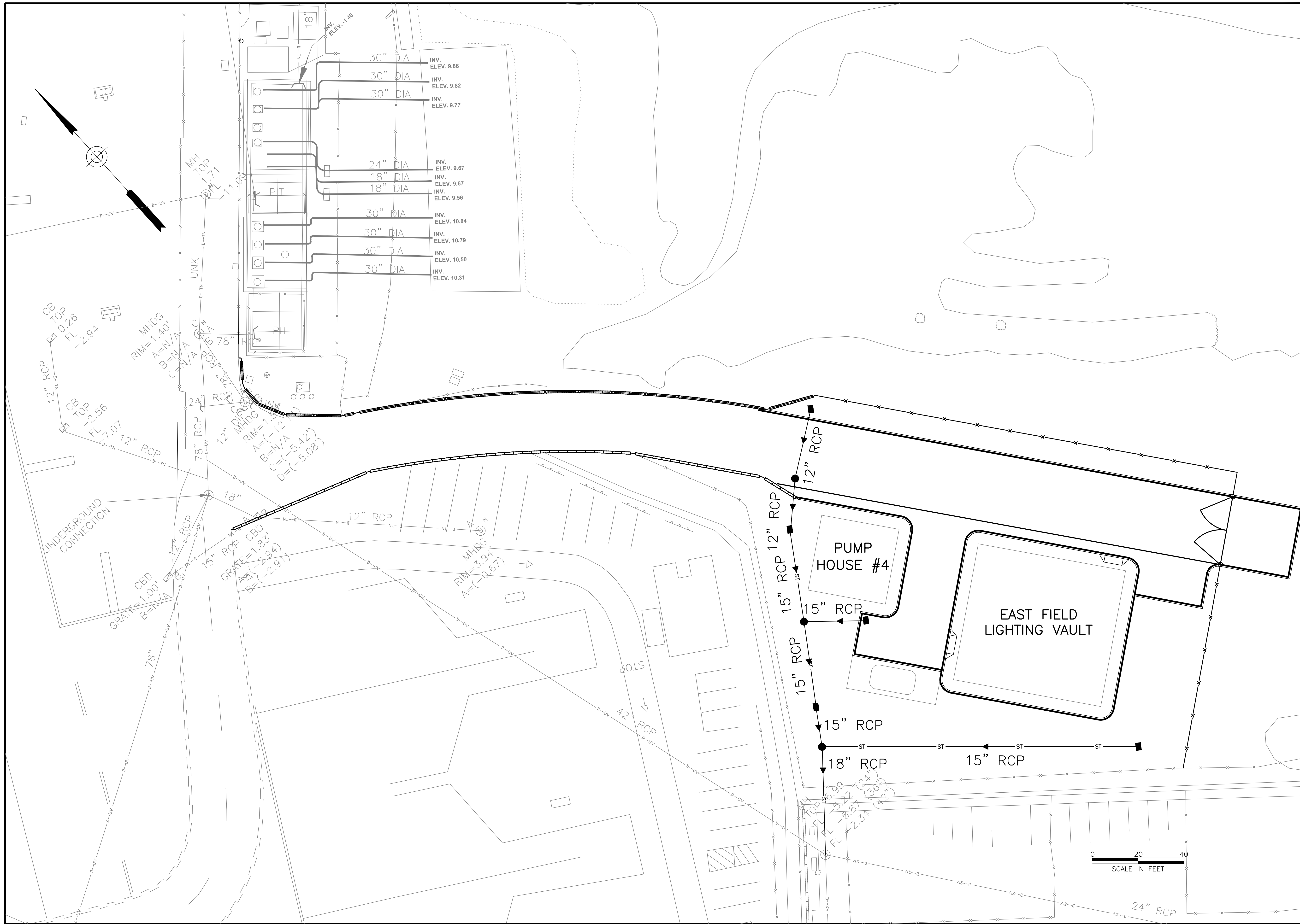
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07/15/2013**

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ENGINEERING DEPARTMENT			
LAGUARDIA AIRPORT			

CIVIL

Title
REPLACEMENT OF THE EAST FIELD LIGHTING VAULT

UTILITY PLAN

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I.POLYANSKIY/A.WASTI A.WASTI R.MALEKI
Designed by Drawn by Checked by

Date **07/15/2013**
WORK ORDER No. # X (TBD)
Contract Number **LGA - 124.223**

Drawing Number **C007**
Number **PID# 12801327**



30% SUBMISSION
07/15/2013

No.	Date	Revision	Approved

ENGINEERING DEPARTMENT

LAGUARDIA
AIRPORT

CIVIL

Title
REPLACEMENT OF THE EAST FIELD
LIGHTING VAULT

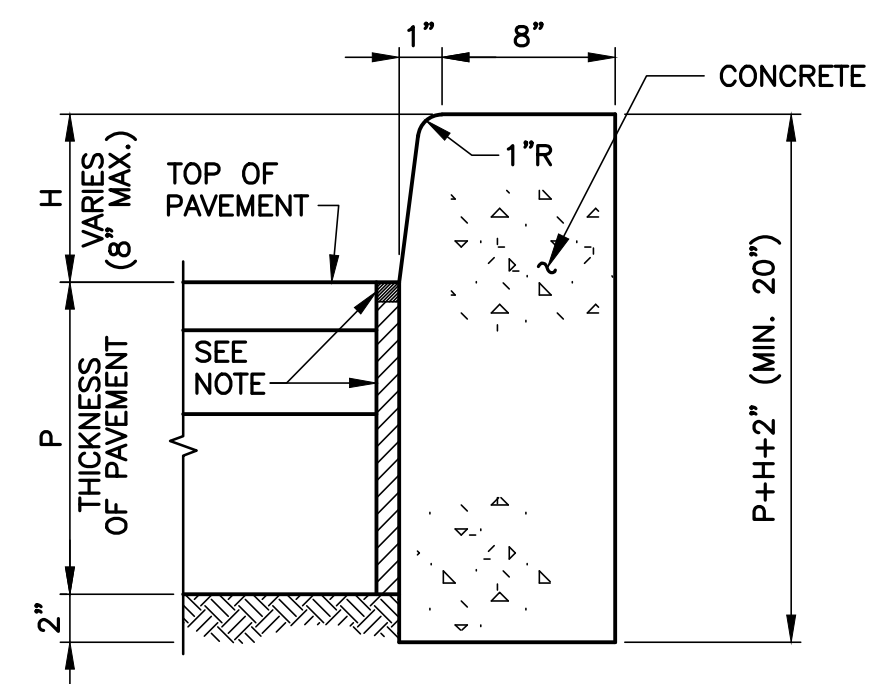
DETAILS
-1-

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I.POLYANSKIY/A.WASTI E. LIU R.MALEKI
Designed by Drawn by Checked by

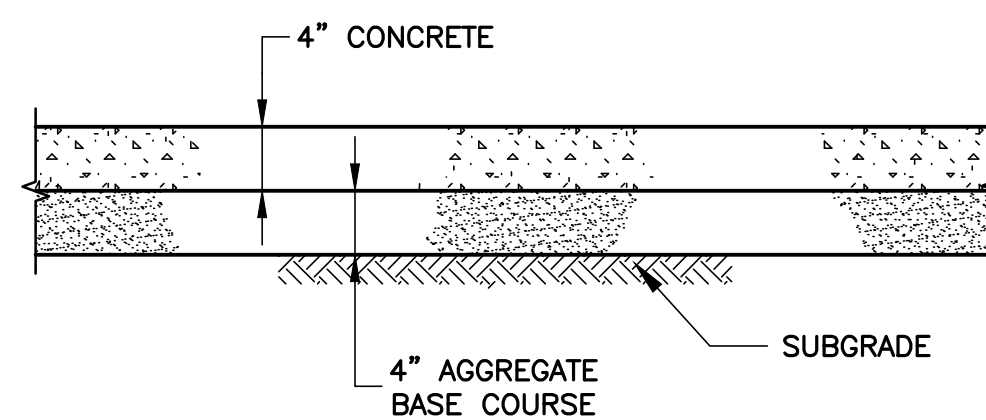
Date 07/15/2013
WORK ORDER No. # X (TBD)
Contract LGA - 124.223
Number

Drawing Number **C008**
PID# 12801327



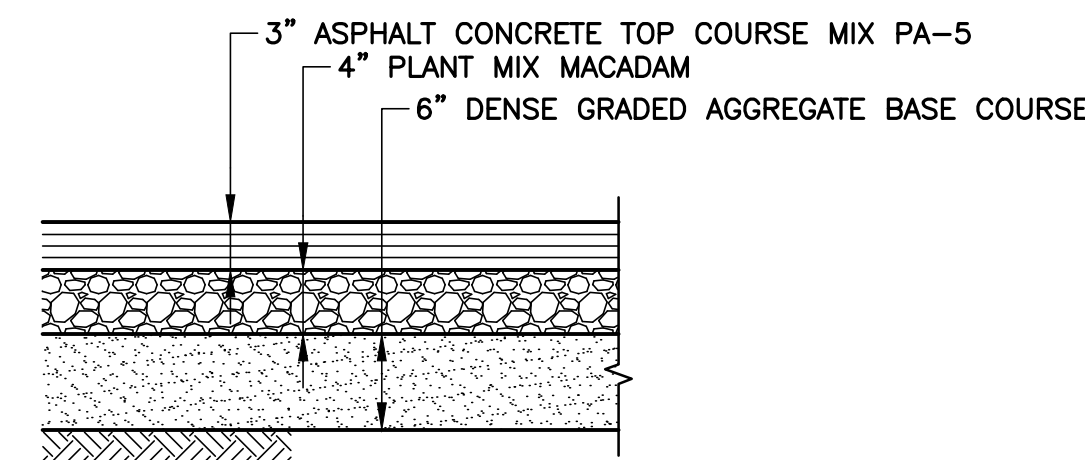
CONCRETE CURB-TYPE I
(NON-MOUNTABLE)

NOTE: PROVIDE 1/2" x 1/2" JOINT SEALANT AND 1/2" PREMOLDED JOINT FILLER WHEN CONCRETE CURB IS PLACED AGAINST RIGID PAVEMENT

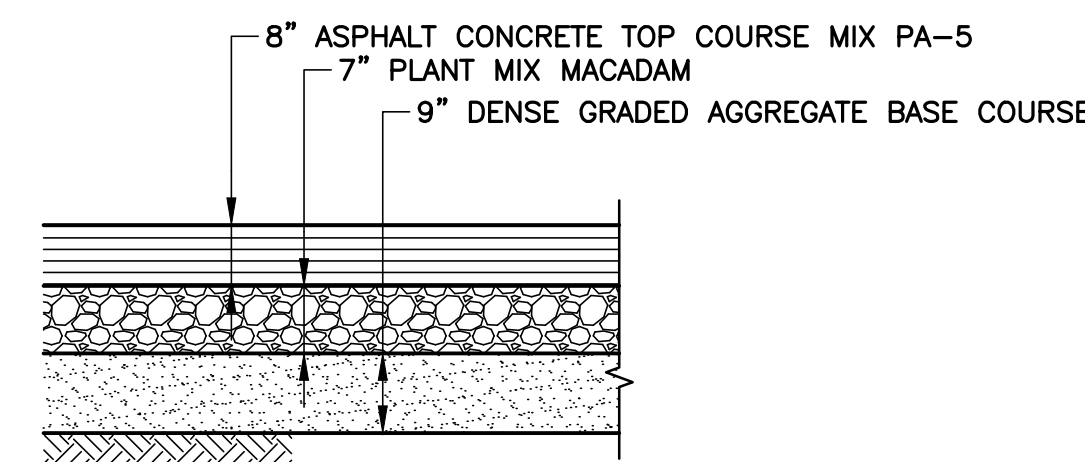


NOTE:
THE LAYOUT OF EXPANSION AND SCORED JOINTS IN THE SIDEWALK AREA SHALL BE SUBMITTED BY THE CONTRACTOR FOR APPROVAL BY THE ENGINEER, UNLESS OTHERWISE SPECIFIED.

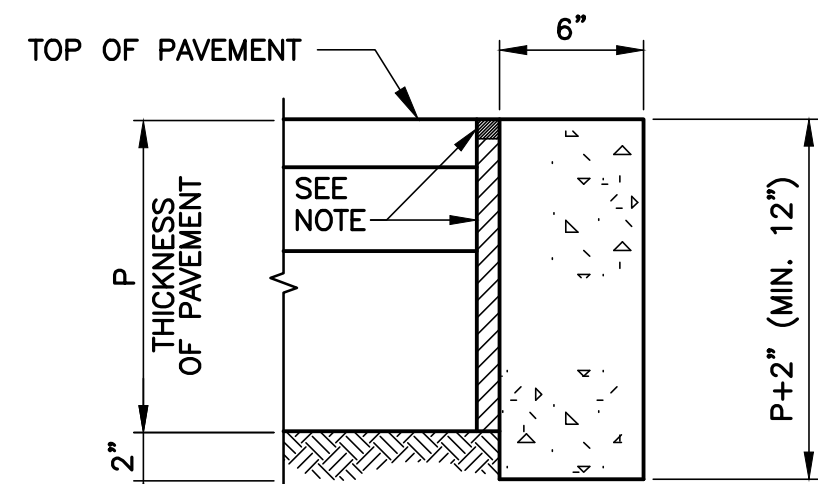
CONCRETE SIDEWALK/ISLAND DETAIL
N.T.S.



ASPHALT CONCRETE PAVEMENT
ROADWAY

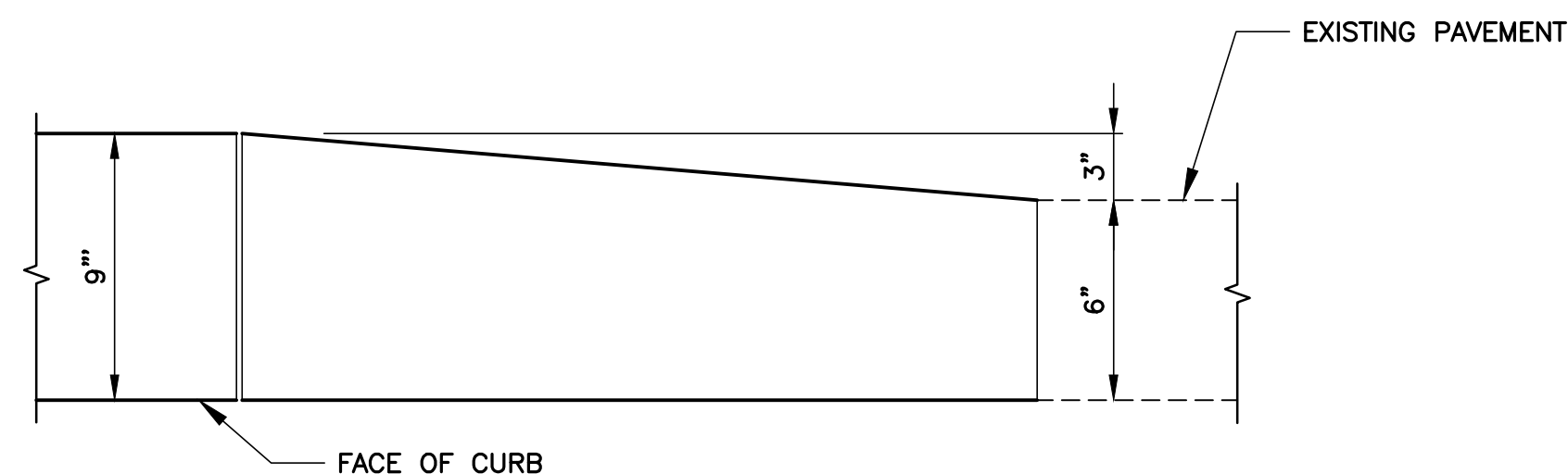


ASPHALT CONCRETE PAVEMENT
APRON

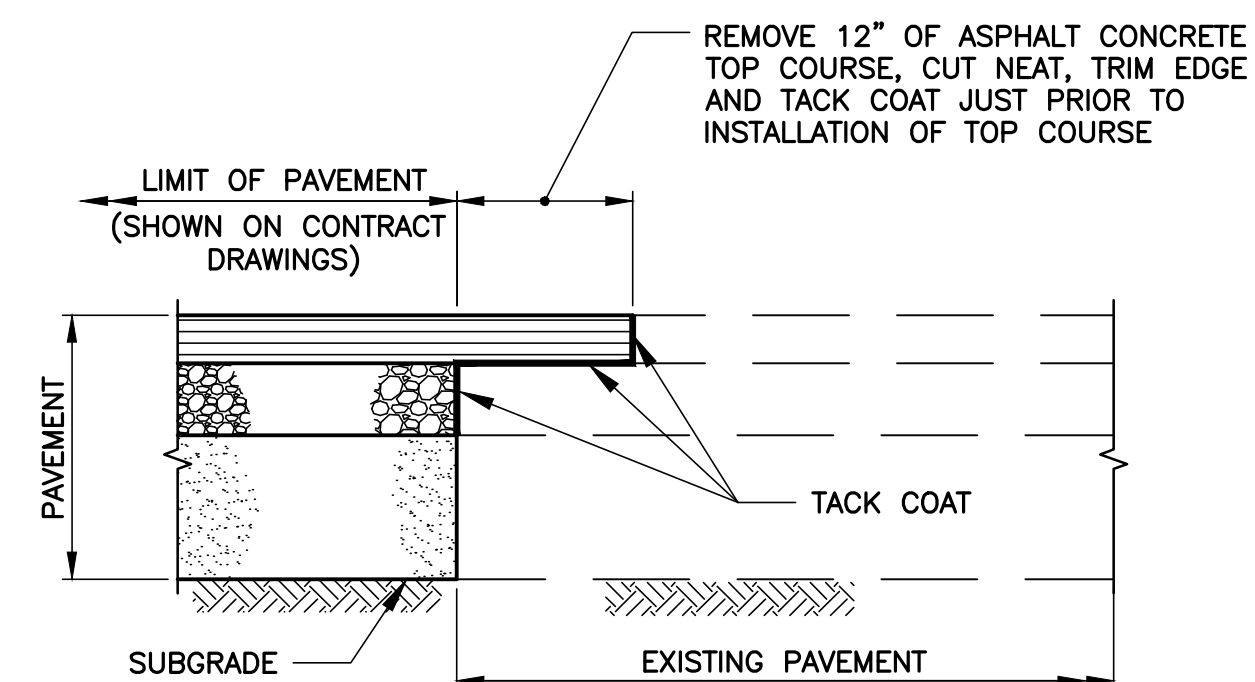


CONCRETE CURB-TYPE III
(HEADER)

NOTE: PROVIDE 1/2" x 1/2" JOINT SEALANT AND 1/2" PREMOLDED JOINT FILLER WHEN CONCRETE CURB IS PLACED AGAINST RIGID PAVEMENT

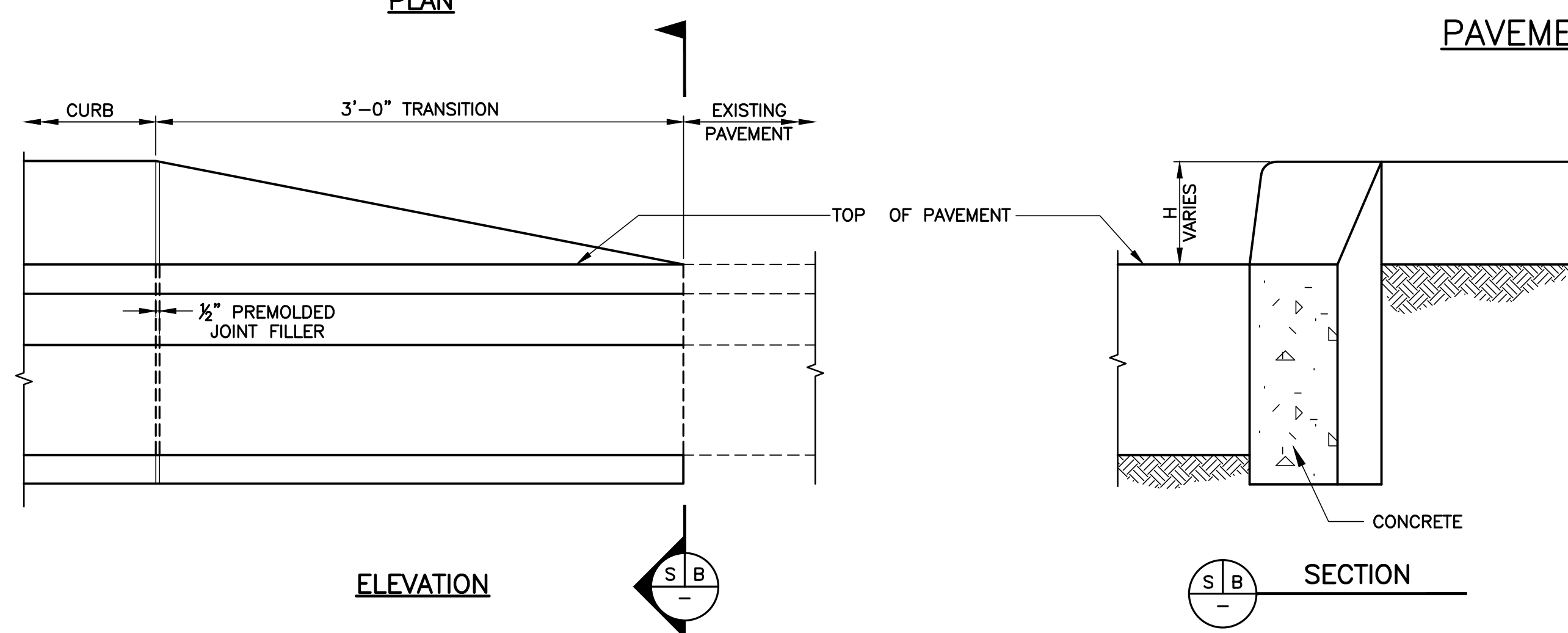


PLAN



PAVEMENT MEETING EXISTING PAVEMENT
N.T.S.

CONCRETE CURB DETAILS
N.T.S.



ELEVATION

SECTION

CURB TRANSITION
(CONCRETE CURB DETAILS)
N.T.S.

30% SUBMISSION
07/15/2013

No.	Date	Revision	Approved

ENGINEERING DEPARTMENT

LAGUARDIA
AIRPORT

CIVIL

Title
REPLACEMENT OF THE EAST FIELD
LIGHTING VAULT

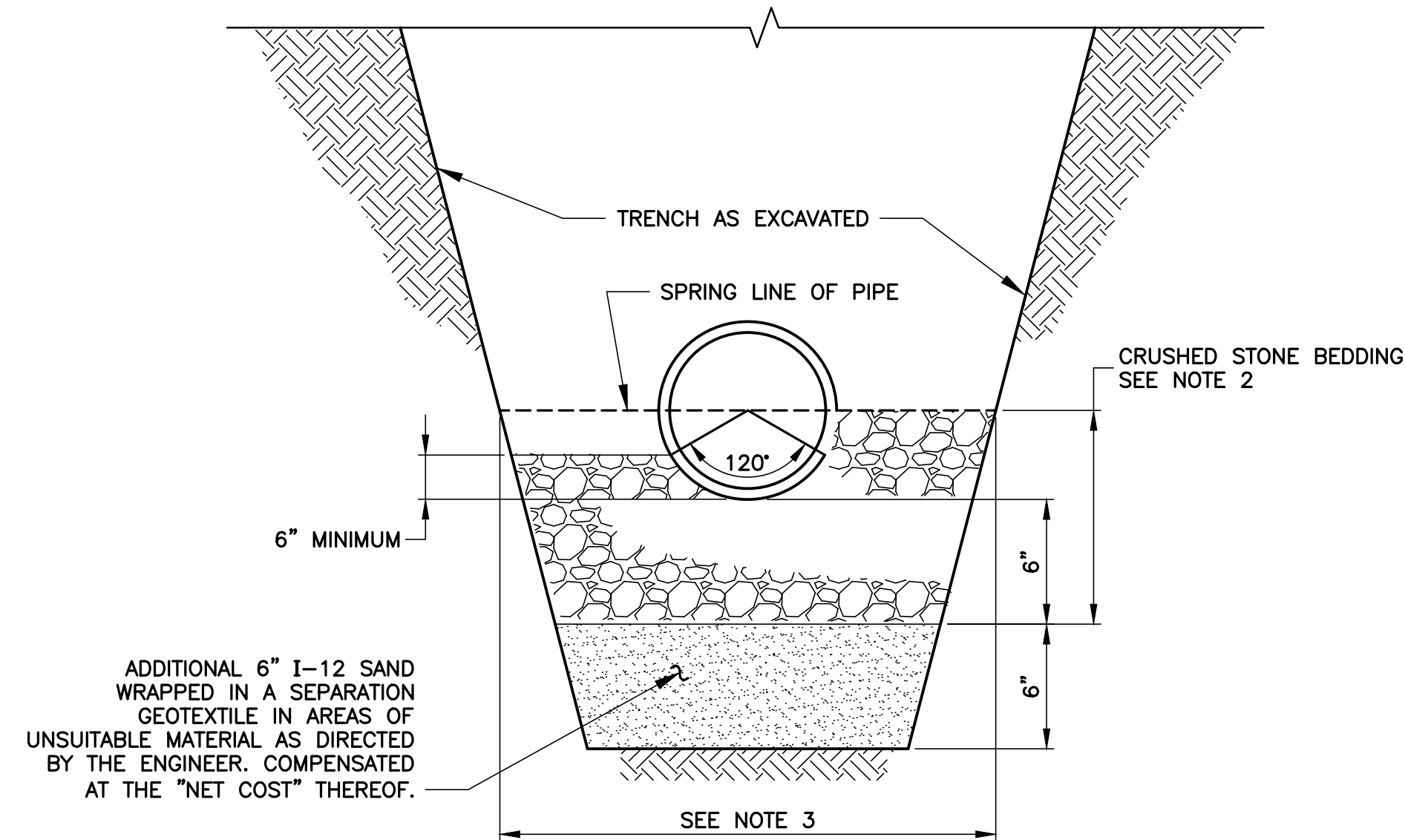
DETAILS
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I.POLYANSKIY/A.WASTI E. LIU R.MALEKI
Designed by Drawn by Checked by

Date 07/15/2013
WORK ORDER No. # X (TBD)
Contract
Number LGA - 124.223

Drawing
Number **C009**
PID# 12801327

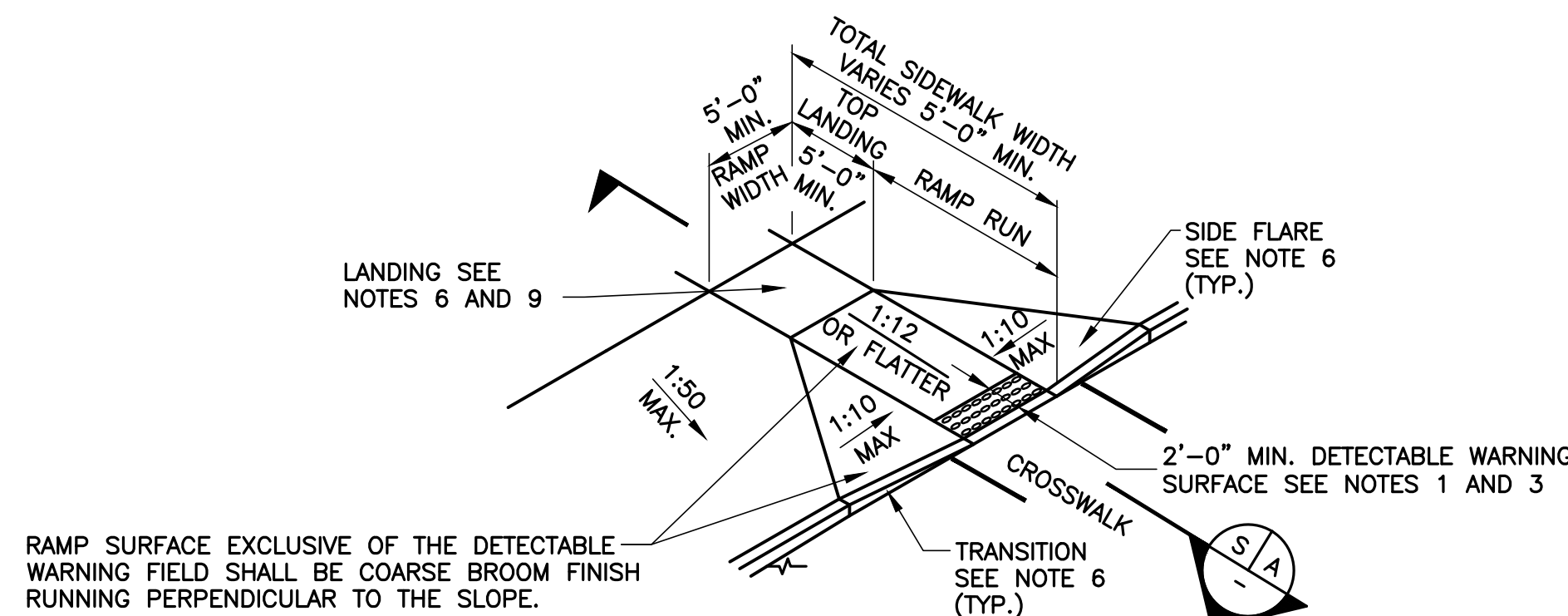


NOTES:

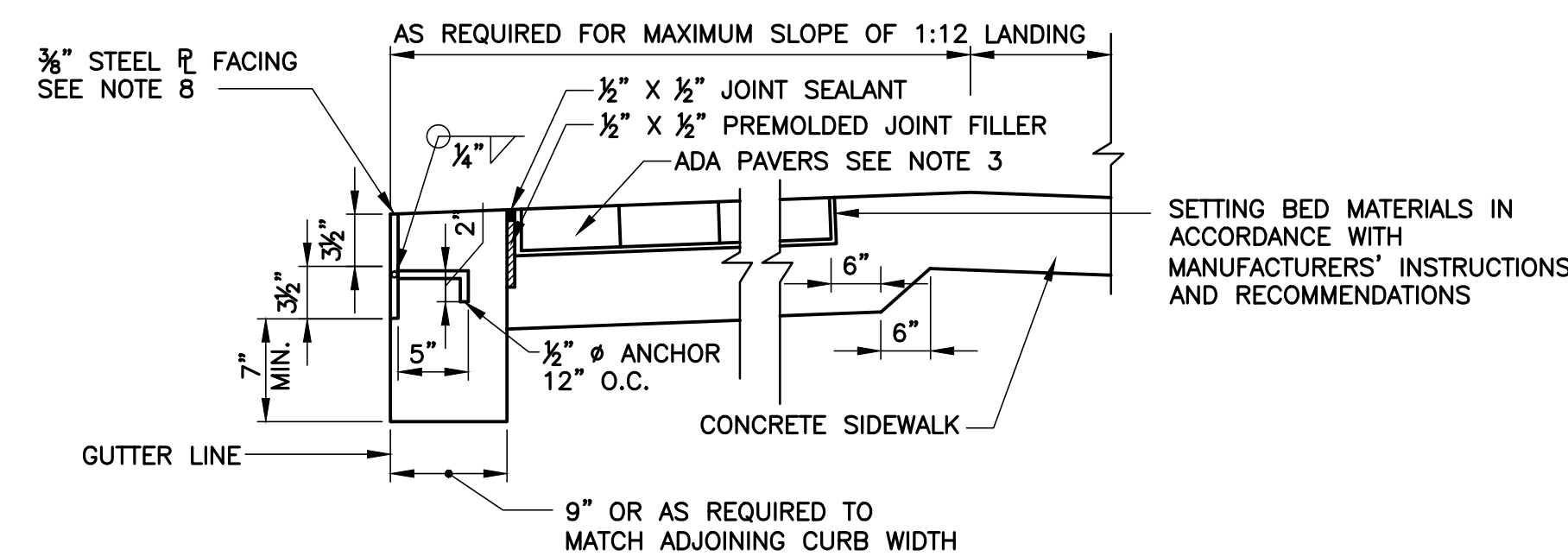
- TRENCH SHALL BE BACKFILLED TO THE SURROUNDING EXISTING GROUND ELEVATION OR PAVEMENT SUBGRADE, WHICHEVER IS LOWER.
- FOR FLEXIBLE PIPE BRING CRUSHED STONE TO SPRING LINE OF PIPE.
- PIPE OUTSIDE DIAMETER + 2'-0" FOR PIPES UP TO AND INCLUDING 18" INSIDE DIAMETER, PIPE OUTSIDE DIAMETER + 3'-0" FOR PIPES OVER 18" INSIDE DIAMETER AND STRUCTURES.
- GEOTEXTILE SHALL HAVE AN APPARENT OPENING SIZE OF LESS THAN 0.6 MILLIMETER AND PERMEABILITY GREATER THAN 10⁻³ CENTIMETER PER SECOND

BEDDING DETAIL

(STORM DRAINAGE, WATER SUPPLY AND SANITARY SEWER SYSTEMS)
N.T.S.



PLAN



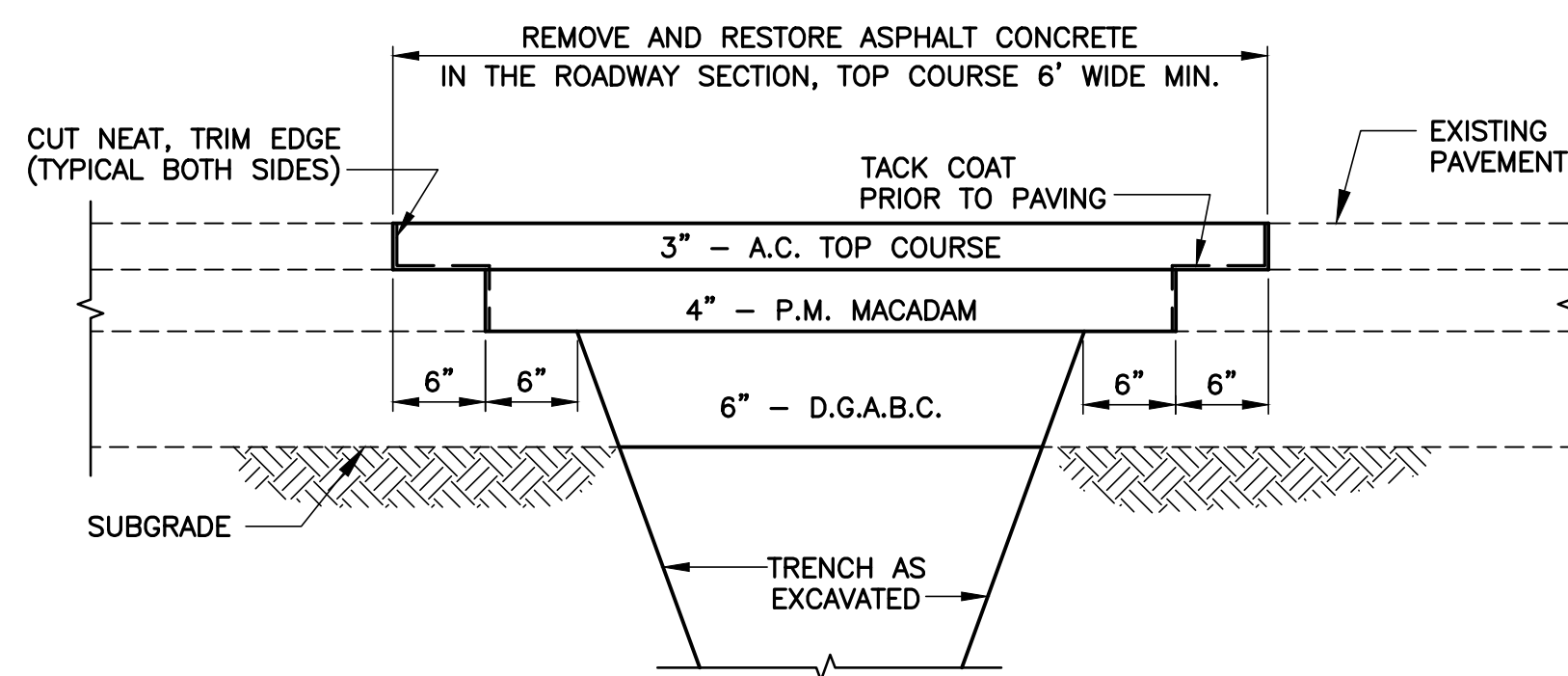
SECTION
NOT TO SCALE

SIDEWALK CURB RAMP DETAILS

NOT TO SCALE

NOTES:

- LOCATION OF DETECTABLE WARNINGS: DETECTABLE WARNINGS SHALL BE LOCATED SO THAT THE EDGE OF THE WARNING FIELD NEAREST TO THE ROADWAY OR STREET SURFACE IS 6" TO 9" FROM THE EDGE OF THE ROADWAY/STREET, OR FROM THE FACE OF THE DROPPED CURB. WHERE A DROPPED CURB CONTINUES ACROSS THE BOTTOM OF THE SIDEWALK CURB RAMP, THE DETECTABLE WARNINGS SHALL EXTEND THE FULL WIDTH OF THE CURB RAMP OR FLUSH SURFACE.
- DOMES ALIGNMENT: DOMES SHALL BE ALIGNED ON A SQUARE GRID IN THE PREDOMINANT DIRECTION OF TRAVEL (SEE 'DOME DETAIL').
- DETECTABLE WARNING SURFACE SHALL BE ONE OF THE FOLLOWING OR APPROVED EQUAL:
 - STEP-SAFE AS MANUFACTURED BY TRANSPO INDUSTRIES, NEW ROCHELLE, NY
 - ADA PAVERS AS MANUFACTURED BY WHITACRE-GREER BRICK, ALLIANCE, OHIO
- THE COLOR OF THE DETECTABLE WARNING SURFACE SHALL BE DARK GRAY UNLESS OTHERWISE SHOWN ON THE CONTRACT DRAWINGS.
- THE MAXIMUM CROSS SLOPE OF CURB RAMPS SHALL BE 2 PERCENT. CURB RAMP SURFACES SHALL GENERALLY LIE IN CONTINUOUS PLANES WITH A MINIMUM SURFACE WARP.
- WHEN NOT PRACTICAL TO PROVIDE A LANDING THAT IS AT LEAST (5') WIDE (MEASURED FROM THE TOP OF THE RAMP TO THE BACK OF THE SIDEWALK), THE LENGTH OF THE FLARES SHALL BE TWELVE (12) TIMES THE CURB HEIGHT MEASURED ALONG THE CURB LINE (SEE 'PARALLEL CURB RAMPS' DETAIL).
- RAMP TRANSITIONS BETWEEN WALKS, GUTTERS, OR STREETS SHALL BE FLUSH AND FREE OF ABRUPT VERTICAL CHANGES (1/4" MAX).
- REQUIRED WHERE STEEL FACED CURB IS SHOWN ON CONTRACT DRAWINGS. MATERIALS SHALL BE SIMILAR TO STEEL FACED CURB, SEE DETAIL. EXPOSED SURFACE OF STEEL SHALL BE GROUND SMOOTH.
- LANDINGS SHALL HAVE A MINIMUM CLEAR DIMENSION OF A 5' BY 5' SQUARE. THE MAXIMUM CROSS SLOPE AT LANDINGS IS 2% IN ANY DIRECTION.



FLEXIBLE PAVEMENT RESTORATION

N.T.S.

30% SUBMISSION
07/15/2013

No.	Date	Revision	Approved

ENGINEERING DEPARTMENT

LAGUARDIA
AIRPORT

CIVIL

Title
REPLACEMENT OF THE EAST FIELD
LIGHTING VAULT

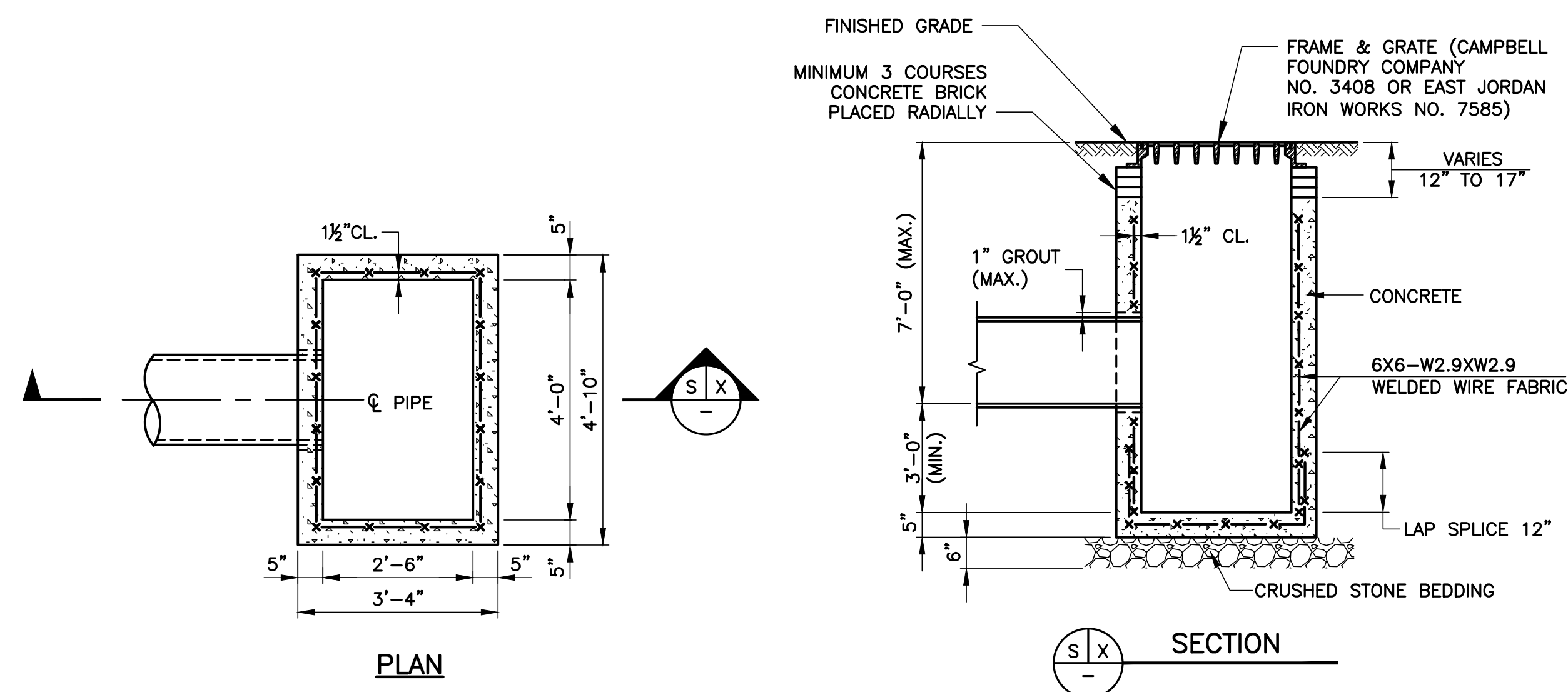
DETAILS
-3-

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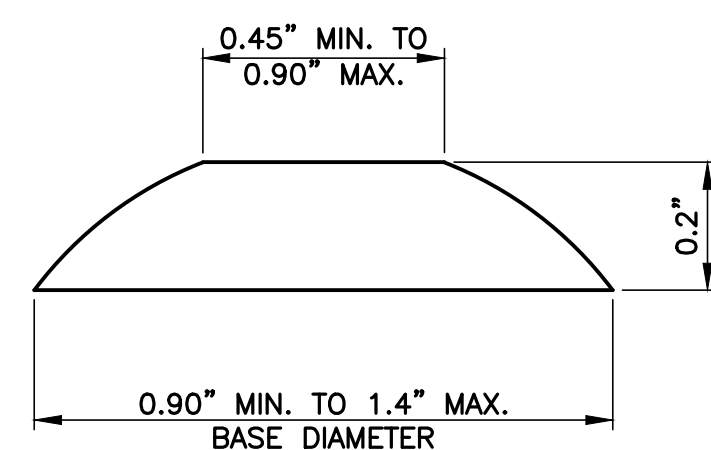
I.POLYANSKIY/A.WASTI E. LIU R.MALEKI
Designed by Drawn by Checked by

Date 07/15/2013
WORK ORDER No. # X (TBD)
Contract LGA - 124.223
Number

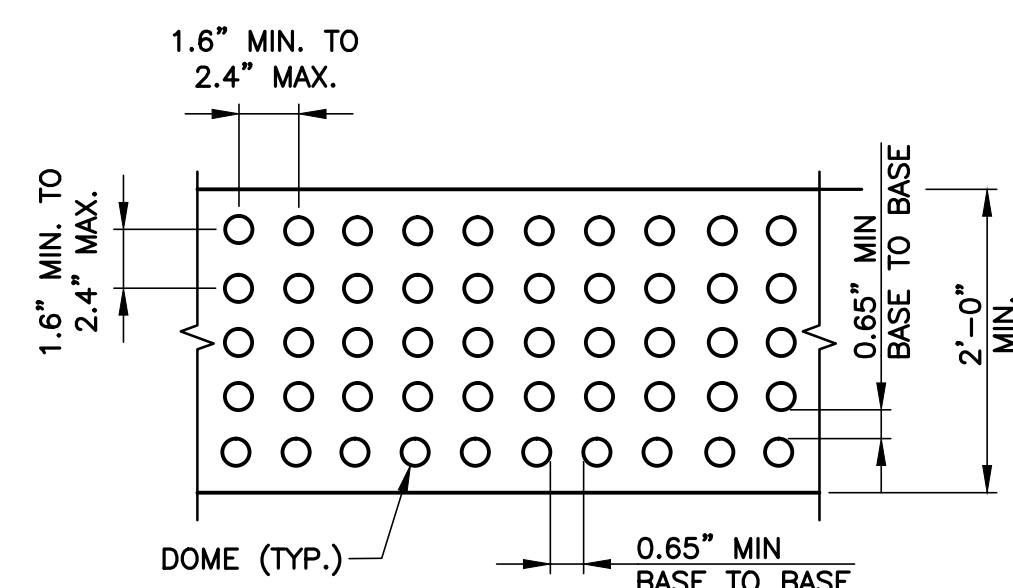
Drawing Number **C010**
PID# 12801327



CATCH BASIN TYPE II PRE-CAST
N.T.S.



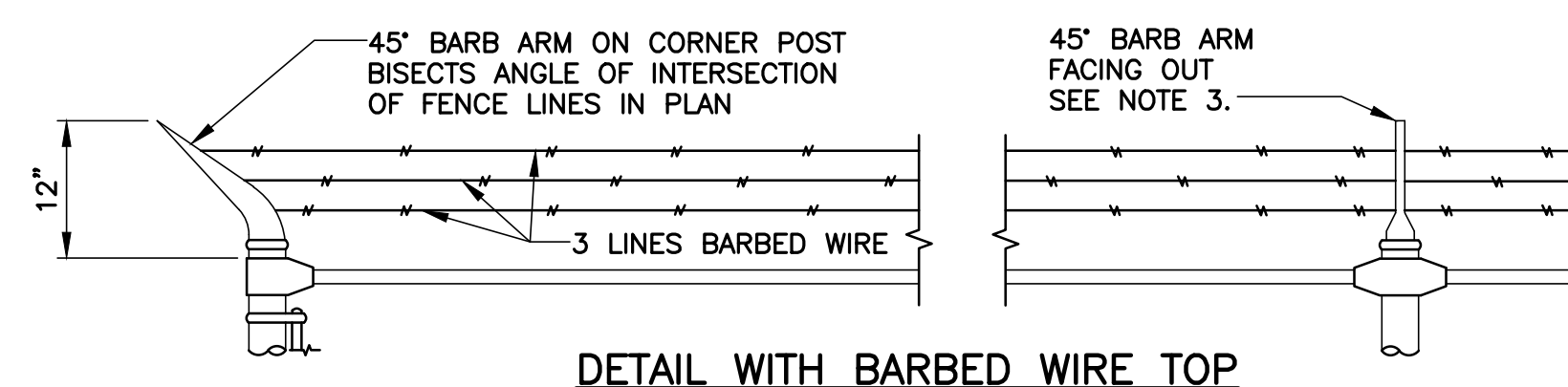
DOME SECTION



DOME SPACING

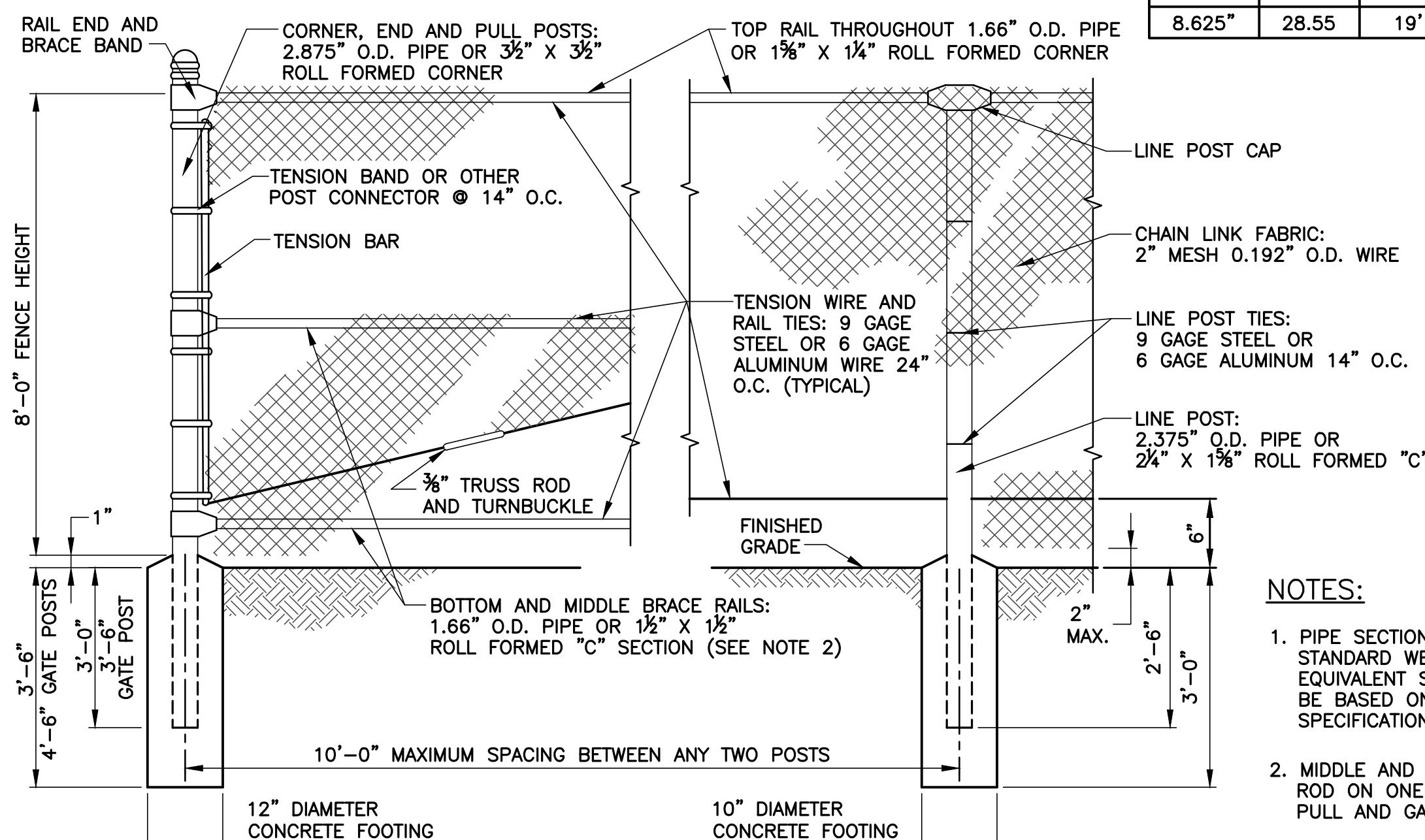
DOME DETAIL
NOT TO SCALE

SIDEWALK DETECTABLE WARNING SURFACE DETAILS
NOT TO SCALE



DETAIL WITH BARBED WIRE TOP

GATE POSTS			
PIPE SIZE	WEIGHT LB/FT	SWING GATE OPENINGS	
NOM. O.D. PIPE		SINGLE GATE	DOUBLE GATE
2.875"	5.79	UP THRU 6'	UP THRU 12'
4.000"	9.11	7' THRU 13'	13' THRU 26'
6.625"	18.97	14' THRU 18'	27' THRU 36'
8.625"	28.55	19' THRU 32'	37' THRU 64'



TERMINAL POST DETAIL

LINE POST DETAIL

EXISTING CHAIN LINK FENCE
NOT TO SCALE

NOTES:

- PIPE SECTIONS SHOWN ARE ASTM F1083 FOR STANDARD WEIGHT (SCHEDULE 40) PIPE. EQUIVALENT STEEL SECTIONS FOR FRAME SHALL BE BASED ON PIPE SECTION SHOWN (SEE SPECIFICATIONS).
- MIDDLE AND BOTTOM BRACE RAILS AND BRACE ROD ON ONE BAY EACH SIDE OF CORNER, END, PULL AND GATE POSTS ONLY.

30% SUBMISSION
07/15/2013

No.	Date	Revision	Approved
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ENGINEERING DEPARTMENT

LAGUARDIA
AIRPORT

CIVIL

Title
REPLACEMENT OF THE EAST FIELD
LIGHTING VAULT

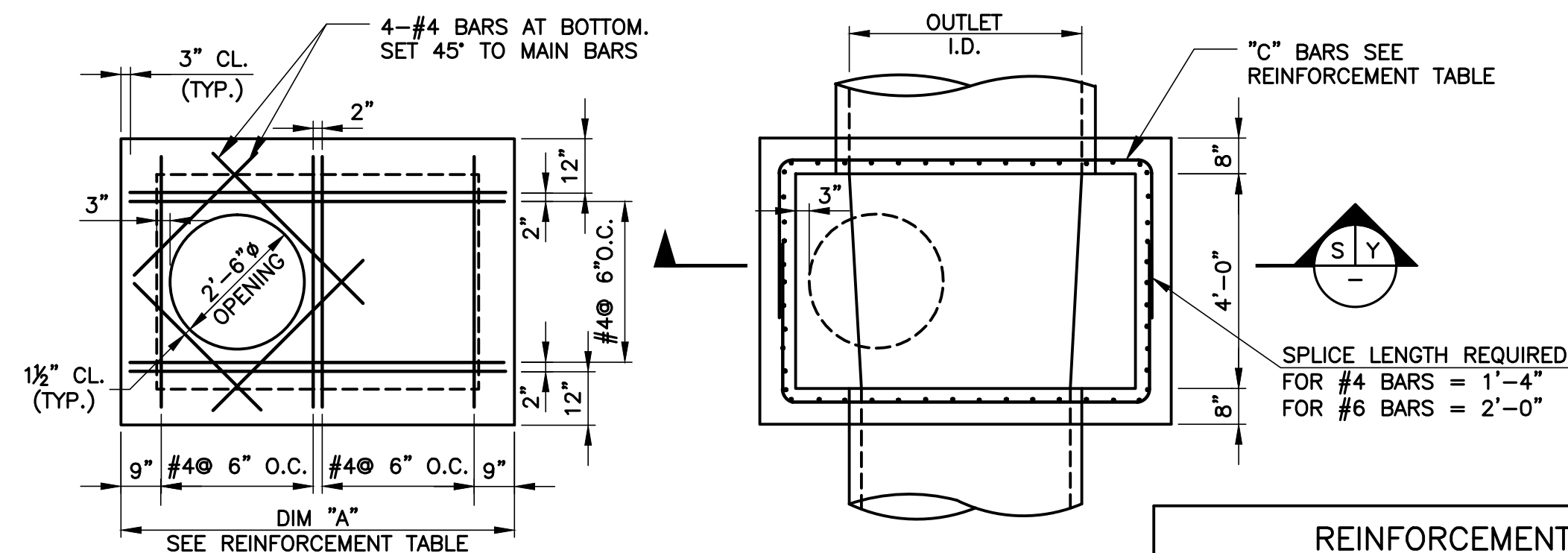
**DETAILS
-4-**

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I.POLYANSKIY/A.WASTI E. LIU R.MALEKI
Designed by Drawn by Checked by

Date 07/15/2013
WORK ORDER No. # X (TBD)
Contract Number LGA - 124.223

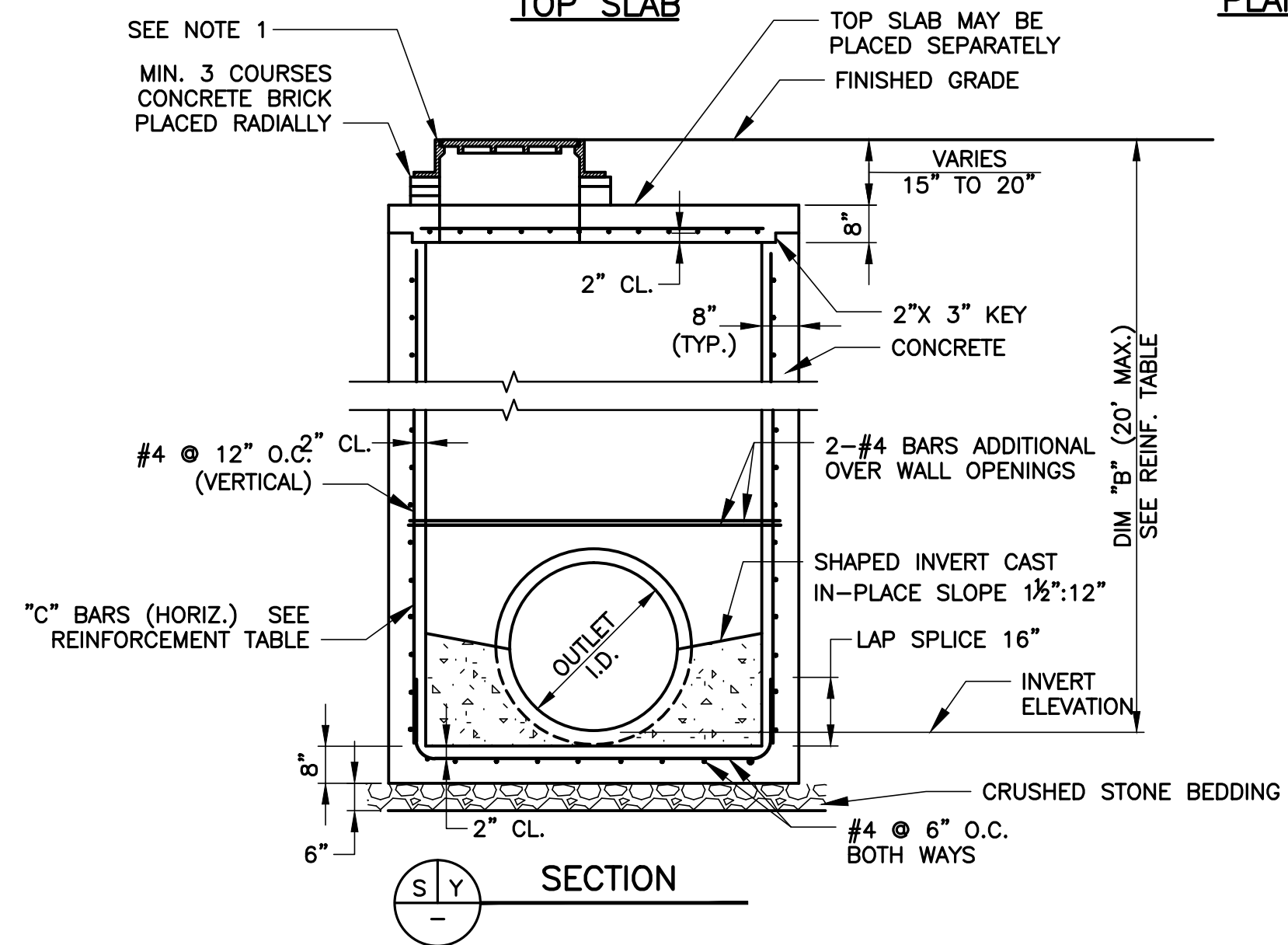
Drawing Number **C011**
PID# 12801327



REINFORCEMENT TABLE			
OUTLET I.D.	DIM. "A"	"C" BARS	
		DIM. "B" ≤ 9'-0"	DIM. "B" ≥ 9'-0"
42"	6'-0"	#4 @ 12" O.C.	#4 @ 12" O.C.
48"	7'-0"	#4 @ 12" O.C.	#4 @ 8" O.C.
54"	7'-6"	#4 @ 10" O.C.	#4 @ 6" O.C.
60"	8'-0"	#4 @ 9" O.C.	#6 @ 12" O.C.
66"	9'-0"	#4 @ 6" O.C.	#6 @ 8" O.C.

NOTES:

1. FRAME & COVER SHALL BE CAMPBELL FOUNDRY CATALOG NO. 1012B OR EAST JORDAN IRON WORKS NO. 1234. FOR SANITARY SEWER MANHOLE FURNISH VENTED COVER AND CAST ALUMINUM SILT BUCKET.
2. OPENINGS IN WALLS FOR PIPE SHALL BE CAST-IN OR CUT CLEANLY WITHOUT PERCUSSION TO A MAXIMUM DIAMETER OF O.D. +3". THE SPACE BETWEEN PIPE AND WALL SHALL THEN BE FILLED WITH GROUT, OR AN APPROVED JOINT INSERT ASSEMBLY.
3. WHEN LIFTING THE PRE-CAST MANHOLE, A BAR SHALL BE PLACED HORIZONTALLY THROUGH LIFTING HOLES PROVIDED AT THE TIME OF MANUFACTURE. THE LIFTING DEVICES SHALL BE PLACED ONLY ON THAT BAR. THE BAR AND HOLES SHALL BE FILLED WITH GROUT AFTER MANHOLE IS IN POSITION.
4. FOR SUBSURFACE DRAIN, WHERE APPLICABLE, SEE DETAIL.



MANHOLE TYPE 'A' PRE-CAST
N.T.S.

30% SUBMISSION
07/15/2013

No.	Date	Revision	Approved

ENGINEERING DEPARTMENT

**LAGUARDIA
AIRPORT**

ARCHITECTURE

Title
**REPLACEMENT OF THE EAST FIELD
LIGHTING VAULT**

**EAST FIELD
LIGHTING VAULT
PROPOSED FLOOR PLAN**

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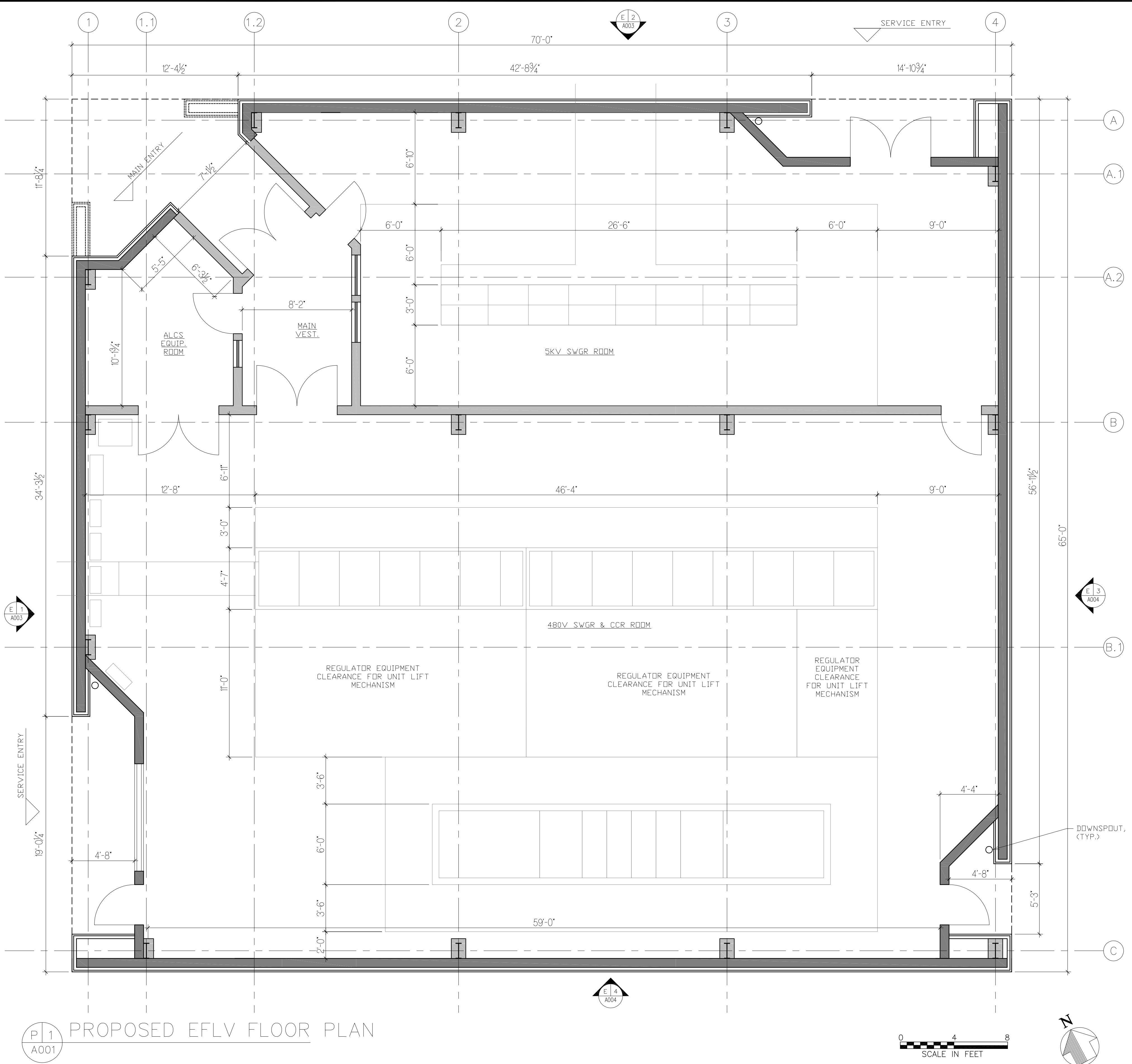
M. PRINCE M. PRINCE S. TING
Designed by Drawn by Checked by

Date 07/15/2013

WORK ORDER No. # X (TBD)
Contract Number **LGA - 124.223**

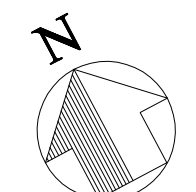
Drawing Number **A001**

PID# 12801327



**P 1
A001** PROPOSED EFLV FLOOR PLAN

0 4 8
SCALE IN FEET



30% SUBMISSION
07/15/2013

No.	Date	Revision	Approved

ENGINEERING DEPARTMENT			
LAGUARDIA AIRPORT			

ARCHITECTURE

Title
REPLACEMENT OF THE EAST FIELD LIGHTING VAULT

**EAST FIELD LIGHTING VAULT
PROPOSED ROOF PLAN**

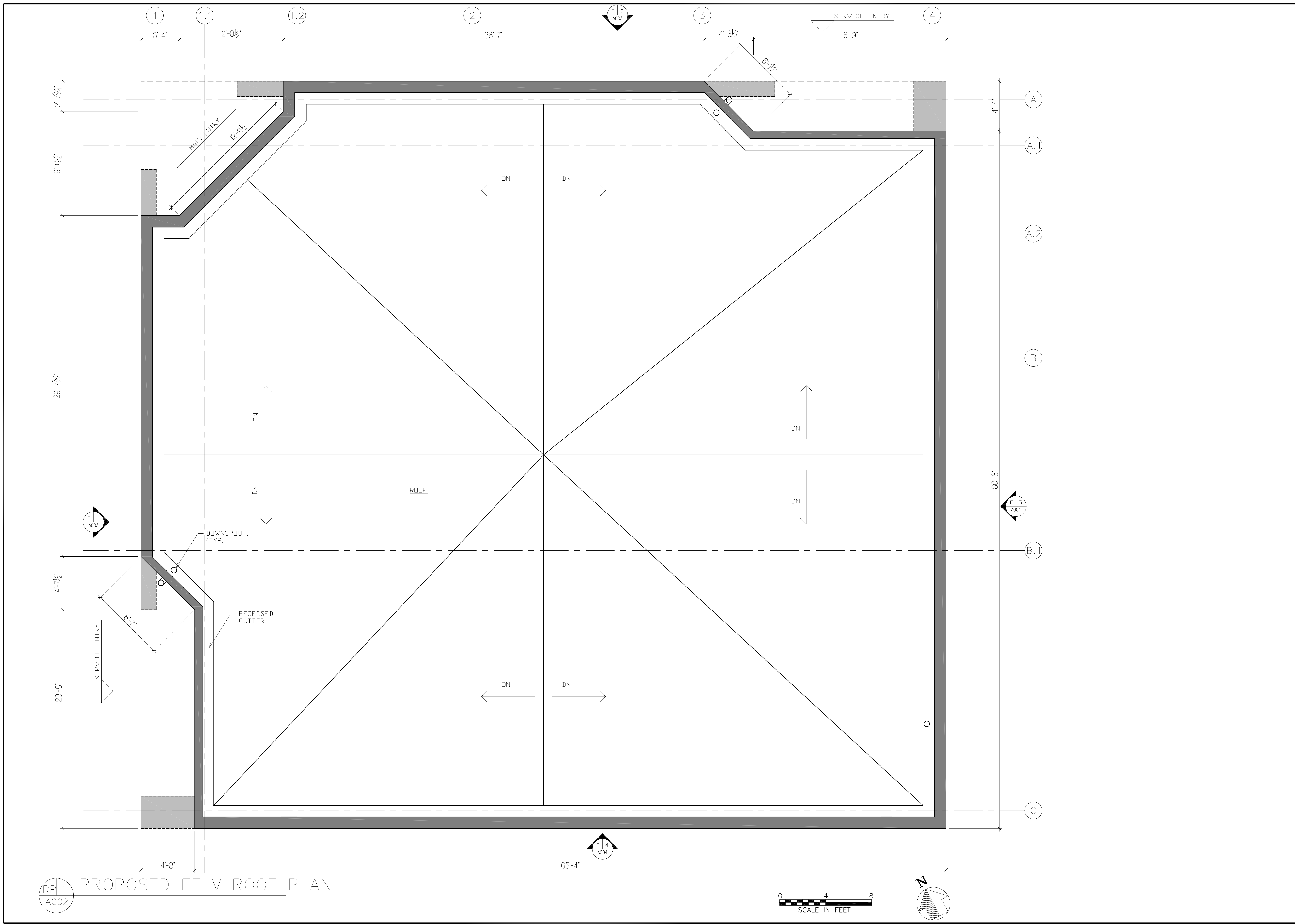
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M. PRINCE M. PRINCE S. TING
Designed by Drawn by Checked by

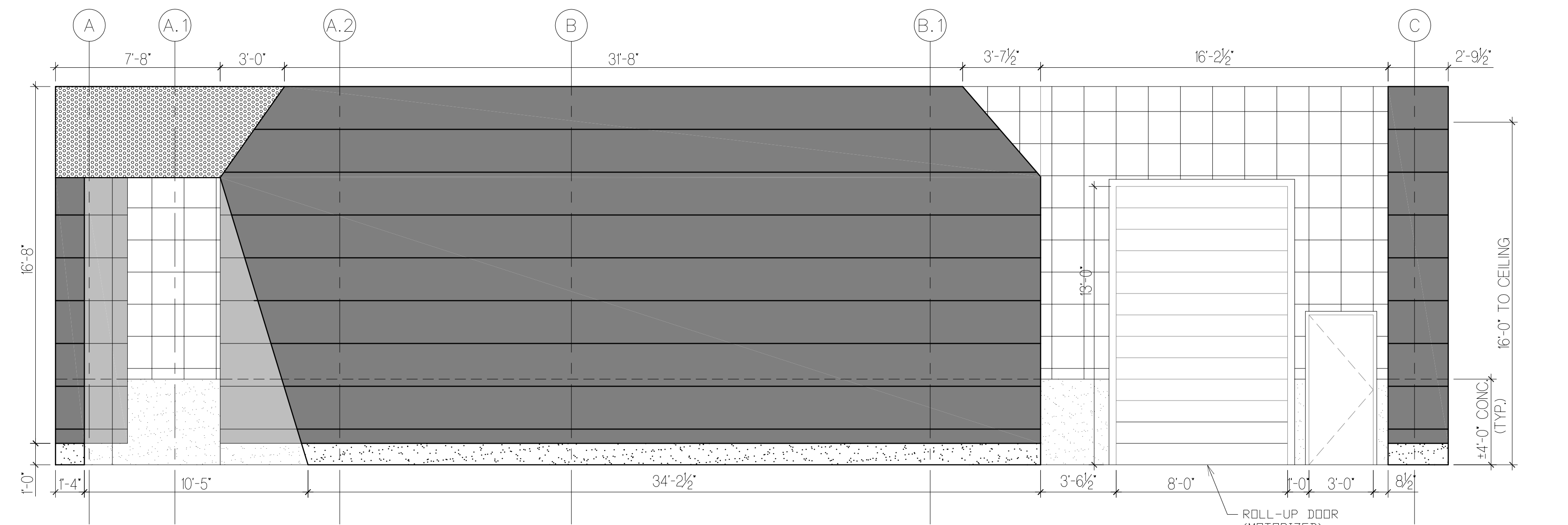
Date 07/15/2013

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Contract Number **LGA - 124.223**

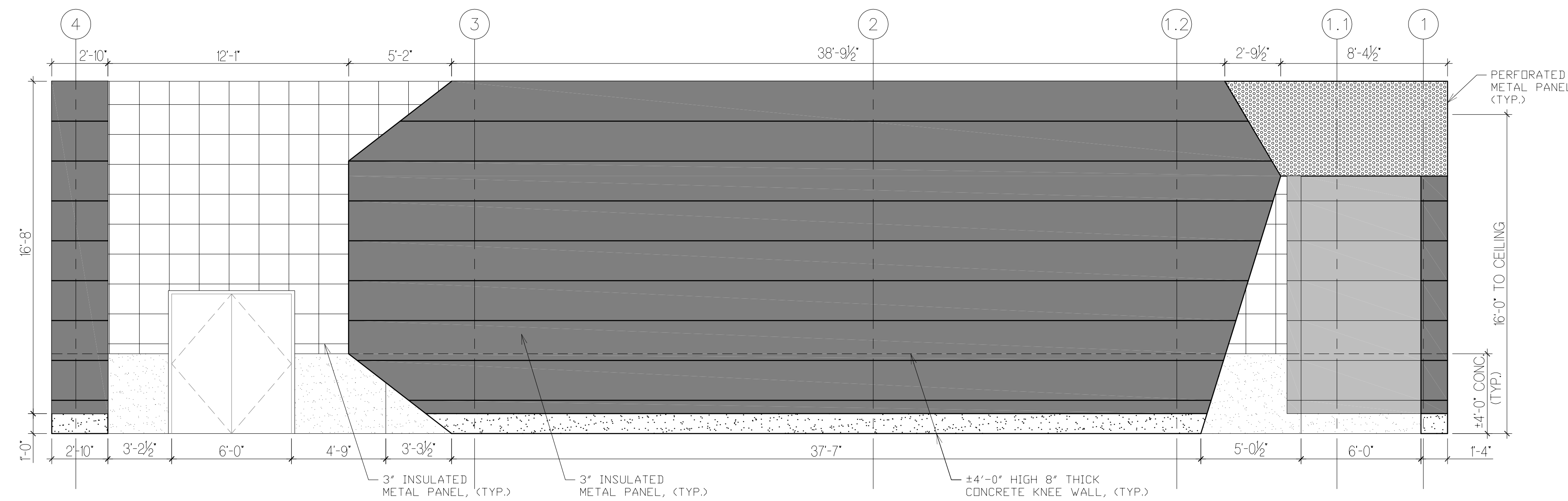
Drawing Number **A002**
PID# 12801327



30% SUBMISSION
07/15/2013



E 1 PROPOSED EFLV WEST ELEVATION
A003



E 2 PROPOSED EFLV NORTH ELEVATION
A003



No.	Date	Revision	Approved

ENGINEERING DEPARTMENT

**LAGUARDIA
AIRPORT**

ARCHITECTURE

Title
**REPLACEMENT OF THE EAST FIELD
LIGHTING VAULT**

**EAST FIELD
LIGHTING VAULT
PROPOSED ELEVATIONS
#1**

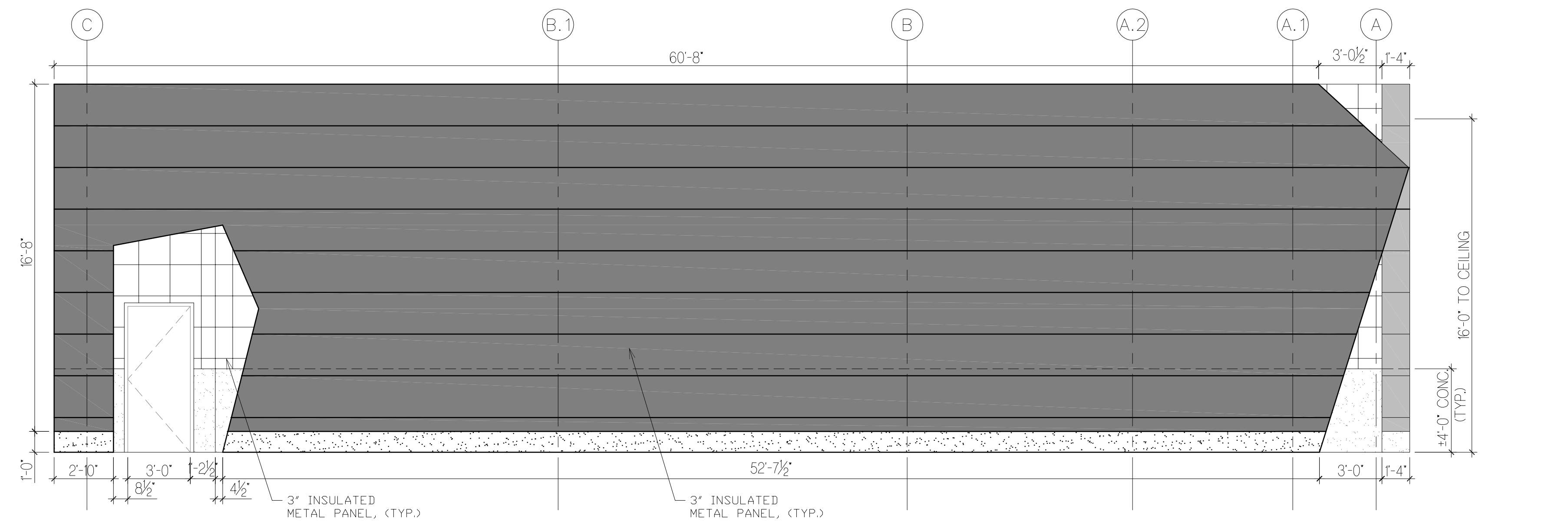
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M. PRINCE M. PRINCE S. TING
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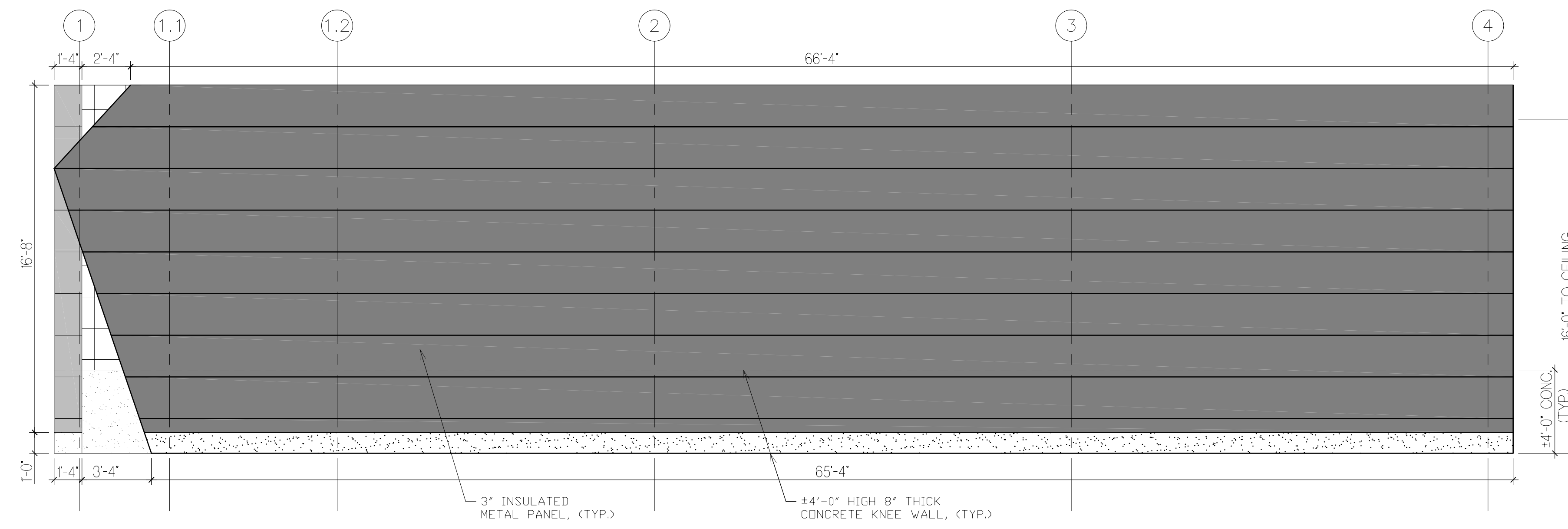
Date 07/15/2013
WORK ORDER No. # X (TBD)
Contract Number **LGA - 124.223**

Drawing Number **A003**
PID# 12801327

30% SUBMISSION
07/15/2013



E 3
A004 **PROPOSED EFLV EAST ELEVATION**



E 4
A004 **PROPOSED EFLV SOUTH ELEVATION**

No.	Date	Revision	Approved

ENGINEERING DEPARTMENT			
LAGUARDIA AIRPORT			

ARCHITECTURE

Title
REPLACEMENT OF THE EAST FIELD LIGHTING VAULT

**EAST FIELD LIGHTING VAULT
PROPOSED ELEVATIONS
#2**

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M. PRINCE	M. PRINCE	S. TING
Designed by	Drawn by	Checked by

Date 07/15/2013

WORK ORDER No. # X (TBD)
Contract Number **LGA - 124.223**

Drawing Number **A004**
PID# 12801327

STRUCTURAL NOTES:

GENERAL NOTES:

- ALL STRUCTURAL WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE REQUIREMENTS OF NEW YORK CITY BUILDING CODE, LATEST EDITION.
- CHECK AND COORDINATE DIMENSIONS, CLEARANCES, INCLUDING OPENINGS FOR PIPES, DUCTS AND CONDUITS, ETC. WITH THE WORK OF OTHER TRADES.
- FOR LOCATION OF LIGHTING VAULT, REFERENCE LINES, COORDINATES AND ELEVATION DATUM SEE CIVIL CONTRACT DRAWINGS

REINFORCED CONCRETE NOTES:

- STRUCTURAL CONCRETE DESIGN AND CONCRETING PRACTICES SHALL CONFORM WITH ACI 318-05, "AMERICAN CONCRETE INSTITUTE, BUILDING CODE REQUIREMENTS FOR REINFORCED CONCRETE". DETAILS SHALL BE IN ACCORDANCE WITH ACI-315, LATEST EDITION, "MANUAL OF STANDARD PRACTICE FOR DETAILING REINFORCED CONCRETE STRUCTURES" UNLESS OTHERWISE NOTED ON THE DRAWINGS.
- CONCRETE FOR WALLS, FOOTINGS, PIERS, AND SLAB ON GRADE SHALL BE CATEGORY IV AS PER TABLE 2 OF THE SPECIFICATION SECTION 03301 WITH A MINIMUM COMPRESSIVE STRENGTH OF 4000 psi AT 28 DAYS. CONCRETE FOR HOUSEKEEPING PADS SHALL BE CATEGORY VI AS PER TABLE 2 OF THE SPECIFICATION SECTION 03301 WITH A MINIMUM COMPRESSIVE STRENGTH OF 4000 psi AT 28 DAYS. CONCRETE ON TOP OF ROOF METAL DECK SHALL BE CATEGORY VI LIGHT WEIGHT CONCRETE WITH 115 PCF DENSITY AND A MINIMUM COMPRESSIVE STRENGTH OF 4000 psi AT 28 DAYS.
- ALL REINFORCING STEEL SHALL BE DEFORMED BARS CONFORMING TO ASTM A615 GRADE 60 (Fy=60 KSI). REINFORCING BARS IN THE GARAGE SLAB ON GRADE (10" SLAB ON PLAN, DWG. S0201) SHALL BE EPOXY COATED CONFORMING TO ASTM A775. FABRICATION AND JOBSITE HANDLING OF EPOXY COATED REINFORCING BARS SHALL CONFORM TO ASTM D3963. WELDED WIRE FABRIC SHALL CONFORM TO REQUIREMENTS OF ASTM A185.
- ALL REINFORCEMENT SHALL BE SECURELY HELD IN PLACE WHILE PLACING CONCRETE. IF REQUIRED, PROVIDE ADDITIONAL BARS OR STIRRUPS TO FURNISH SUPPORT FOR ALL BARS.
- REINFORCING BARS SHALL BE LAPPED AS SPECIFICALLY DETAILED ON THE DRAWINGS. WHERE NOT SPECIFICALLY INDICATED ON THE DRAWINGS, REINFORCING BARS SHALL BE LAPPED USING THE TENSION SPLICE LENGTHS IN THE SCHEDULE ON THE DRAWING SOXX.
- MINIMUM CONCRETE COVER SHALL BE:
 - CONCRETE CAST AGAINST AND PERMANENTLY EXPOSED TO EARTH 3"
 - CONCRETE EXPOSED TO EARTH OR WEATHER (VERTICAL REINFORCEMENT OF RETAINING WALLS, TOP SLAB ON GRADE BARS, ETC.)
 - #5 AND SMALLER 1 1/2"
 - #6 AND LARGER 2"
- PROVIDE CONSTRUCTION JOINTS IN ACCORDANCE WITH ACI 318-05, CHAPTER 6.4 AND TYPICAL CONSTRUCTION JOINTS DETAILS ON SOXX DRAWING. SUBMIT SHOP DRAWINGS SHOWING CONSTRUCTION JOINT LOCATIONS ALONG WITH THE SEQUENCE OF POURS FOR THE ENGINEER'S REVIEW AND APPROVAL. RETAINING WALL CONSTRUCTION JOINTS SHALL BE LOCATED SO AS TO PROVIDE A 60 FOOT MAXIMUM LENGTH OF CONCRETE PLACEMENT.
- NO HORIZONTAL CONSTRUCTION JOINTS WILL BE PERMITTED IN WALLS OR SLAB ON GRADE UNLESS SPECIFICALLY SHOWN ON THE DRAWINGS OR APPROVED IN WRITING BY THE ENGINEER. VERTICAL CONSTRUCTION JOINTS IN WALLS SHALL BE USED ONLY WITH PRIOR APPROVAL OF THE ENGINEER. LOCATION OF CONSTRUCTION JOINTS SHALL BE AS APPROVED BY THE ENGINEER.
- CONCRETE PLACEMENT, TEST AND CURING SHALL BE PERFORMED IN ACCORDANCE WITH SPECIFICATION SECTION 03301.
- VERIFY DIMENSIONS AND LOCATIONS OF ALL OPENINGS, PIPE SLEEVES, CONDUITS, ANCHOR BOLTS AND EMBEDMENTS ETC. AS REQUIRED BY OTHER TRADES BEFORE CONCRETE IS PLACED.
- CONCRETE SLAB IN GRADE AND EQUIPMENT PADS SHALL RECEIVE TROWEL FINISH.

STRUCTURAL STEEL NOTES:

- DESIGN OF STRUCTURAL STEEL AND CONNECTIONS SHALL CONFORM TO THE AMERICAN INSTITUTE OF STEEL CONSTRUCTION, "MANUAL OF STEEL CONSTRUCTION, ALLOWABLE STRESS DESIGN (LATEST EDITION).", AS WELL AS SPECIFICATION SECTION 05120.
- FABRICATION AND ERECTION OF STRUCTURAL STEEL SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE AMERICAN INSTITUTE OF STEEL CONSTRUCTION "CODE OF STANDARD PRACTICE FOR STEEL BUILDINGS AND BRIDGES," ADOPTED EFFECTIVE 1986.
- UNLESS OTHERWISE NOTED:
 - ALL W-SHAPES SHALL CONFORM TO ASTM SPECIFICATION A992 (Fy=50 ksi, Fu=65 ksi)
 - ALL ANGLES USED IN VERTICAL OR HORIZONTAL BRACING SYSTEMS SHALL CONFORM TO ASTM SPECIFICATION A572 GRADE 50 (Fy=50 ksi, Fu=65 ksi)
 - ALL RECTANGULAR AND SQUARE HSS SECTIONS SHALL CONFORM TO ASTM SPECIFICATION A500 GRADE B (Fy=46 ksi, Fu=58 ksi)
 - ALL CHANNELS AND ANGLES (OTHER THEN USED IN BRACING SYSTEMS) SHALL CONFORM TO ASTM SPECIFICATION A36 (Fy=36 ksi, Fu=58 ksi)
 - ALL CONNECTION MATERIAL, PLATES, BASE PLATES SHALL CONFORM TO ASTM SPECIFICATION SECTION A36 (Fy=36 ksi, Fu=58 ksi).
- COMPLETE THE DESIGN OF CONNECTIONS BASED ON DETAILS SHOWN ON CONTRACT DRAWINGS. DESIGN AND DETAILING SHALL BE PERFORMED BY A PROFESSIONAL ENGINEER LICENSED IN NEW YORK STATE. THE CALCULATIONS AND SHOP DRAWINGS SHALL ALSO BEAR THE SIGNATURE AND SEAL OF A PROFESSIONAL ENGINEER LICENSED IN NEW YORK STATE. ALL CALCULATIONS AND SHOP DRAWINGS SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW AND APPROVAL.
- WHEN NOT SPECIFICALLY DETAILED ELSEWHERE ON THE DRAWINGS, ALL BEAM TO BEAM AND BEAM TO COLUMN CONNECTIONS SHALL BE TWO SIDED WEB ANGLE CONNECTIONS AS SHOWN IN TYPICAL SHEAR CONNECTION OF BEAM ON DRAWING SOXX.
- ALTERNATE CONNECTIONS WILL BE ACCEPTED ONLY WITH WRITTEN APPROVAL OF THE ENGINEER, WHO WILL BE THE SOLE JUDGE OF ACCEPTABILITY. CONTRACTOR'S BID SHALL ANTICIPATE THE USE OF THOSE SPECIFIC DETAILS SHOWN ON THE DRAWINGS. IN ANY EVENT, THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE DESIGN OF SUCH ALTERNATE DETAILS WHICH HE PROPOSES.

- WHEN INDICATED AS BOLTED ON THE DRAWINGS, ALL SHOP OR FIELD BOLTED BEAM TO BEAM OR BEAM TO COLUMN CONNECTIONS SHALL BE BEARING TYPE BOLTED CONNECTIONS USING 3/4" DIAMETER A325 N BOLTS IN STANDARD HOLES UNLESS SPECIFICALLY NOTED OTHERWISE ON SECTIONS OR DETAILS. NO CONNECTION SHALL HAVE LESS THAN TWO (2) BOLTS UNLESS OTHERWISE NOTED ON SECTIONS OR DETAILS.
 - ALL BEAMS AND GIRDERS SHALL BE CONNECTED FOR THE REACTION (SHEAR CONNECTION FORCE) DENOTED BY THE SYMBOL "V" ON PLANS. IF NO REACTION IS GIVEN, CONNECTIONS SHALL BE CAPABLE OF DEVELOPING THE UNIFORMLY DISTRIBUTED LOAD CAPACITY OF THE MEMBER USING THE REACTION FROM THE ALLOWABLE LOAD OF BEAM AS TABULATED IN THE MANUAL OF STEEL CONSTRUCTION PART 2.
 - WHEN INDICATED AS BOLTED ON THE DRAWINGS, ALL SHOP OR FIELD BOLTED COLUMN SPLICES, BRACING AND MOMENT CONNECTIONS SHALL BE FRICTION TYPE (SLIP CRITICAL) CLASS A CONNECTIONS USING 3/4" DIAMETER A325 SLIP CRITICAL BOLTS IN STANDARD HOLES.
 - SPLICES SHALL BE ALLOWED ONLY AT LOCATIONS SPECIFICALLY INDICATED ON THE STRUCTURAL DRAWINGS UNLESS APPROVED OTHERWISE BY THE ENGINEER IN WRITING.
 - OVERSIZED OR SLOTTED HOLES SHALL NOT BE USED FOR ANY CONNECTIONS UNLESS SPECIFICALLY INDICATED ON THE DRAWINGS OR APPROVED BY THE ENGINEER IN WRITING.
 - UNLESS NOTED ON DRAWINGS, ALL FILLET WELDS INDICATED SHALL MEET THE MINIMUM WELD SIZE SPECIFIED BY THE AISC MANUAL OF STEEL CONSTRUCTION.
 - ALL WELDING SHALL BE PERFORMED BY QUALIFIED WELDERS IN ACCORDANCE WITH A.W.S. D1.1 SPECIFICATIONS, LATEST EDITIONS. ALL WELDING ELECTRODES SHALL CONFORM TO A.W.S. A5.1 GRADE E-70. BARE ELECTRODES AND GRANULAR FLUX SHALL CONFORM TO A.W.S. A5.17, F70 A.W.S. FLUX CLASSIFICATION.
 - PROVIDE WELDED STIFFENER PLATES ON BOTH SIDES OF THE WEB OF BEAMS AT POINTS OF CONCENTRATED LOAD.
 - CUTS, HOLES, COPING, ETC. REQUIRED FOR WORK OF OTHER TRADES SHALL BE SHOWN ON THE SHOP DRAWINGS AND MADE IN SHOP. CUTS OR BURNING OF HOLES IN STRUCTURAL STEEL MEMBERS IN THE FIELD WILL NOT BE PERMITTED.
 - STRUCTURAL DRAWINGS SHALL BE USED IN CONJUNCTION WITH ARCHITECTURAL AND MECHANICAL DRAWINGS AND DRAWINGS RELATED TO OTHER TRADES. CHECK AND COORDINATE DIMENSIONS, CLEARANCES, ETC. WITH WORK OF OTHER TRADES.
 - CAMBER WHERE REQUIRED AS INDICATED BY "C" IN FRAMING PLANS FOLLOWED BY THE ORDINATE, IN INCHES. WHERE NO CAMBER IS INDICATED, MEMBERS SHALL BE FABRICATED AND PLACED WITH NATURAL CAMBER UP.
 - PROVIDE SHELF ANGLES AT COLUMNS, WALLS AND BEAMS AS REQUIRED TO PROVIDE END AND SIDE DECK SUPPORTS. SEE TYPICAL DETAILS ON DWG. SOXX.
 - DOUBLE ANGLE MEMBERS SHALL HAVE LONG LEGS BACK-TO-BACK UNLESS OTHERWISE NOTED. SEE TYPICAL DETAILS FOR CONNECTION.
 - COORDINATE ALL MISCELLANEOUS STEEL DETAILING REQUIREMENTS SHOWN ON THE DRAWINGS BY OTHER DISCIPLINES.
 - STRUCTURAL STEEL FINISH REQUIREMENTS:
 - STEEL MEMBERS FOR GIRT SUPPORT SYSTEMS SHALL BE HOT-DIP GALVANIZED IN ACCORDANCE WITH ASTM A123 OR A153. ALL CONNECTION BOLTS SHALL BE MECHANICALLY GALVANIZED PER ASTM B695.
 - STEEL MEMBERS THAT ARE EXPOSED AND NOT FIREPROOFED SHALL BE PAINTED WITH 3 COAT SYSTEM IN ACCORDANCE WITH SYSTEM S-1S OF SPECIFICATION SECTION #09910. SURFACE PREPARATION SHALL FOLLOW SSPC SP10. SHOP PAINT THE PRIMER AND SECOND COAT, FIELD PAINT THE 3RD COAT. COLOR TO BE DETERMINED BY ENGINEER.
 - STRUCTURAL STEEL THAT ARE IN CONTACT WITH GALVANIZED STEEL SHALL BE PAINTED WITH ZINC-PRIMER COAT OF SYSTEM S-1S PER SPECIFICATION SECTION #09910. SURFACE PREPARATION SHALL FOLLOW SSPC SP-10.
 - GALVANIZED FINISH THAT ARE DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED IN ACCORDANCE WITH ASTM A780, USING ZINC-RICH PAINT CONTAINING A MINIMUM OF 92% ZINC IN THE DRY FILM.
- FOR FIREPROOFING REQUIREMENT, SEE ARCHITECTURAL DRAWINGS.

CONTROLLED INSPECTIONS:

- CONTROLLED INSPECTIONS SHALL BE CONDUCTED BY THE PORT AUTHORITY OF NEW YORK AND NEW JERSEY FOR THE FOLLOWING CONSTRUCTION AND OPERATIONS:
- STEEL CONSTRUCTION:
 - WELDING OF STRUCTURAL STEEL.
 - INSTALLATION AND TENSIONING OF HIGH-STRENGTH BOLTS.
 - CONCRETE CONSTRUCTION:
 - MATERIAL CONFORMANCE TO ACI AND ASTM STANDARDS FOR STRENGTH, CEMENT, AGGREGATES, MIXING WATER, STEEL REINFORCEMENT AND ADMIXTURES.
 - PLACING AND CURING OF ALL CONCRETE.
 - MASONRY CONSTRUCTION:
 - FABRICATION OF PREFABRICATED UNITS.
 - PLACEMENT AND BEDDING OF UNITS, SIZES AND WALL THICKNESS OF MEMBERS, SIZE AND POSITION OF REINFORCEMENT AND PROVISIONS FOR CURING AND PROTECTION OF MASONRY.

METAL DECK NOTES:

- METAL DECK SHALL BE OF THE FOLLOWING TYPE OR APPROVED EQUAL:
 - 1.5" VL20 GALVANIZED COMPOSITE METAL DECK AS MANUFACTURED BY VULCRAFT.
- ALL METAL DECK SHALL BE FABRICATED AND INSTALLED FOR A MINIMUM THREE-SPAN CONDITION. ONE SPAN CONDITIONS ARE PROHIBITED UNLESS SPECIFICALLY CALLED FOR ON THE DRAWINGS. TEMPORARY SHORING SHALL NOT BE USED ON METAL DECK UNLESS APPROVED BY THE ENGINEER OR CALLED OUT ON DRAWINGS.

METAL DECK NOTES:

- METAL DECK SHALL BE OF THE FOLLOWING TYPE OR APPROVED EQUAL:
 - 1.5" VL20 GALVANIZED COMPOSITE METAL DECK AS MANUFACTURED BY VULCRAFT.
- ALL METAL DECK SHALL BE FABRICATED AND INSTALLED FOR A MINIMUM THREE-SPAN CONDITION. ONE SPAN CONDITIONS ARE PROHIBITED UNLESS SPECIFICALLY CALLED FOR ON THE DRAWINGS. TEMPORARY SHORING SHALL NOT BE USED ON METAL DECK UNLESS APPROVED BY THE ENGINEER OR CALLED OUT ON DRAWINGS.
- METAL DECK UNITS SHALL BE FASTENED TO STEEL FRAMING AS FOLLOWS:
 - A - AT ENDS OF DECK UNITS AS WELL AS TO ALL INTERMEDIATE SUPPORTS (GIRDERS AND FILLER BEAMS THAT ARE PERPENDICULAR TO THE DECK DIRECTION)
 - B - TO ALL EDGE BEAMS (BEAMS AT THE PERIMETER OF THE BUILDING, AT THE EDGE OF LARGE OPENINGS SUCH AS STAIR AND ELEVATOR OPENINGS AND MECHANICAL SHAFTS, AT EDGE OF DEPRESSED AREAS)
 - C - TO ALL BEAMS LOCATED ON COLUMN LINES REGARDLESS OF THE BEAM ORIENTATION IN RESPECT TO DECK DIRECTION (INCLUDING BEAMS ON COLUMN LINES THAT ARE PARALLEL TO DECK DIRECTION)
- ALL METAL ROOF DECK UNITS SHALL BE FASTENED TO THE STEEL FRAMEWORK BY WELDS NOT LESS THAN 5/8" DIAMETER SPACED AT 8" ON CENTER MAXIMUM. SIDE LAPS OF ADJACENT UNITS SHALL BE FASTENED BETWEEN SUPPORTS BY SIDE SEAM WELDING OR SIDELAP SCREWS SPACED PER MANUFACTURERS ENGINEERED CALCULATIONS WITH MAXIMUM SPACING OF 18" FOR WELDS AND 12" ON CENTER FOR SCREWS. SEAM WELDS SHALL BE A MINIMUM OF 1-1/2 INCH BY 1/2 INCH.
- OPENINGS WILL NOT BE PERMITTED IN THE ROOF DECK UNLESS SPECIFICALLY CALLED FOR AND DETAILED ON THE SHOP DRAWINGS APPROVED BY THE ENGINEER.
- THE HANGING OF ANY LOADS FROM A ROOF METAL DECK IS STRICTLY PROHIBITED.
- METAL DECK HANGER TABS SHALL NOT BE USED FOR HANGING ANY LOADS (INCLUDING LIGHTWEIGHT CEILING).
- COORDINATE DECK OPENING SIZES AND LOCATIONS WITH ARCHITECTURAL AND MECHANICAL DRAWINGS. PROVIDE HEADER MEMBERS OR REINFORCEMENT AS REQUIRED UNLESS OTHERWISE NOTED ON PLANS OR SECTIONS AND DETAILS.
- ALL COMPOSITE METAL DECK SHALL INCLUDE POUR STOP AND END CLOSURE.

STEEL FRAMING NOTES:

- SYMBOLS:**
 - INDICATES DIRECTION OF 3/4" (TOTAL DEPTH) LIGHT WEIGHT CONCRETE ON TOP OF 1.5" VL20 COMPOSITE METAL DECK.
 - INDICATES VERTICAL BRACING.
 - INDICATES MOMENT CONNECTION. PROVIDE CONNECTION FOR FULL MOMENT CAPACITY OF BEAM UNLESS MOMENT IS INDICATED ON PLAN.
- NOTATION:**
 - V.... INDICATES GRAVITY SHEAR CONNECTION FORCE IN KIPS. IF NO REACTION IS SHOWN, REFER TO STRUCTURAL STEEL NOTES ON SOXX. MINIMUM SHEAR REACTION SHALL BE 6 KIPS.
 - c=.... INDICATES CAMBER AT CENTER OF BEAM SPAN. THE SPECIFIED CAMBER INCLUDES ALL MILL TOLERANCES.
 - M.... INDICATES MOMENT CONNECTION FORCE IN KIP- FEET.
 - P.... INDICATES AXIAL CONNECTION FORCE [TENSION (+) OR COMPRESSION (-)] IN KIPS.

**LEGEND:
(STRUCTURAL DRAWINGS ONLY)**

	PLAN, SECTION, ELEVATION OR DETAIL IDENTIFICATION		COMPACTED FILL MATERIAL
	DRAWING NUMBER ON WHICH SECTION IS SHOWN, OR DASH (-) IF SHOWN ON SAME DRAWING		DENSE GRADED AGGREGATE BASE COURSE
	BOLD LINE = MATERIAL FURNISHED AND INSTALLED IN THIS CONTRACT		EARTH
	CONCRETE INSTALLED IN THIS CONTRACT		

ABBREVIATIONS:

ADD'L	-	ADDITIONAL	DIAG.	-	DIAGONAL
ARCH'L	-	ARCHITECTURAL	DIAM., Ø	-	DIAMETER
B, BOTT.	-	BOTTOM	DO	-	DITTO
BLDG.	-	BUILDING	DWG., DWGS	-	DRAWING(S)
BM	-	BEAM	EA.	-	EACH
C.C.	-	CENTER TO CENTER	EL., ELEV.	-	ELEVATION
CL	-	CENTER LINE	ELECT.	-	ELECTRICAL
COL.	-	COLUMN			
CONC.	-	CONCRETE			



CHIEF STRUCTURAL ENGINEER

30% SUBMISSION
07/15/2013

No.	Date	Revision	Approved

ENGINEERING DEPARTMENT			
LAGUARDIA AIRPORT			

STRUCTURAL

Title
REPLACEMENT OF THE EAST FIELD LIGHTING VAULT

**STRUCTURAL NOTES,
LEGEND AND
ABBREVIATIONS**

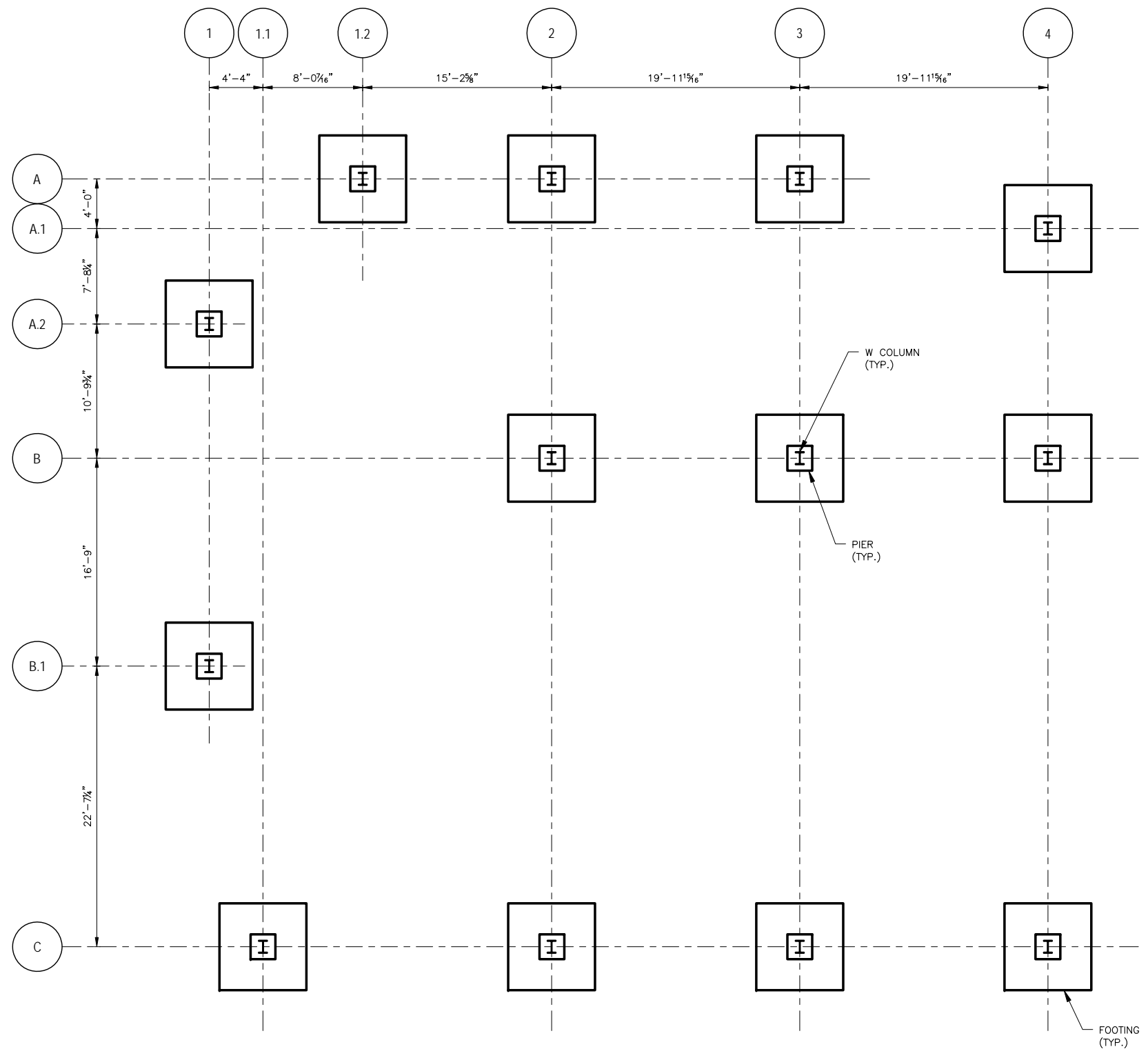
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H.NI	Y.VAYNER	J.CHU
Designed by	Drawn by	Checked by

Date **07/15/2013**

Contract Number **LGA - 124.223**

Drawing Number **S001**
PID# 12801327



FOUNDATION PLAN
 0 6 12
 SCALE IN FEET

Sheet of

THE PORT AUTHORITY OF NY & NJ

CHIEF STRUCTURAL ENGINEER

30% SUBMISSION
07/15/2013

No.	Date	Revision	Approved
ENGINEERING DEPARTMENT			
LAGUARDIA AIRPORT			
STRUCTURAL			
Title			
REPLACEMENT OF THE EAST FIELD LIGHTING VAULT			
FOUNDATION PLAN			

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H.NI	Y.VAYNER	J.CHU
Designed by	Drawn by	Checked by
Date	07/15/2013	
Contract Number	LGA - 124.223	
Drawing Number	S002	
PID#	12801327	

30% SUBMISSION
07/15/2013

No.	Date	Revision	Approved

ENGINEERING DEPARTMENT

LAGUARDIA
AIRPORT

STRUCTURAL

Title
REPLACEMENT OF THE EAST FIELD
LIGHTING VAULT

GROUND FLOOR PLAN

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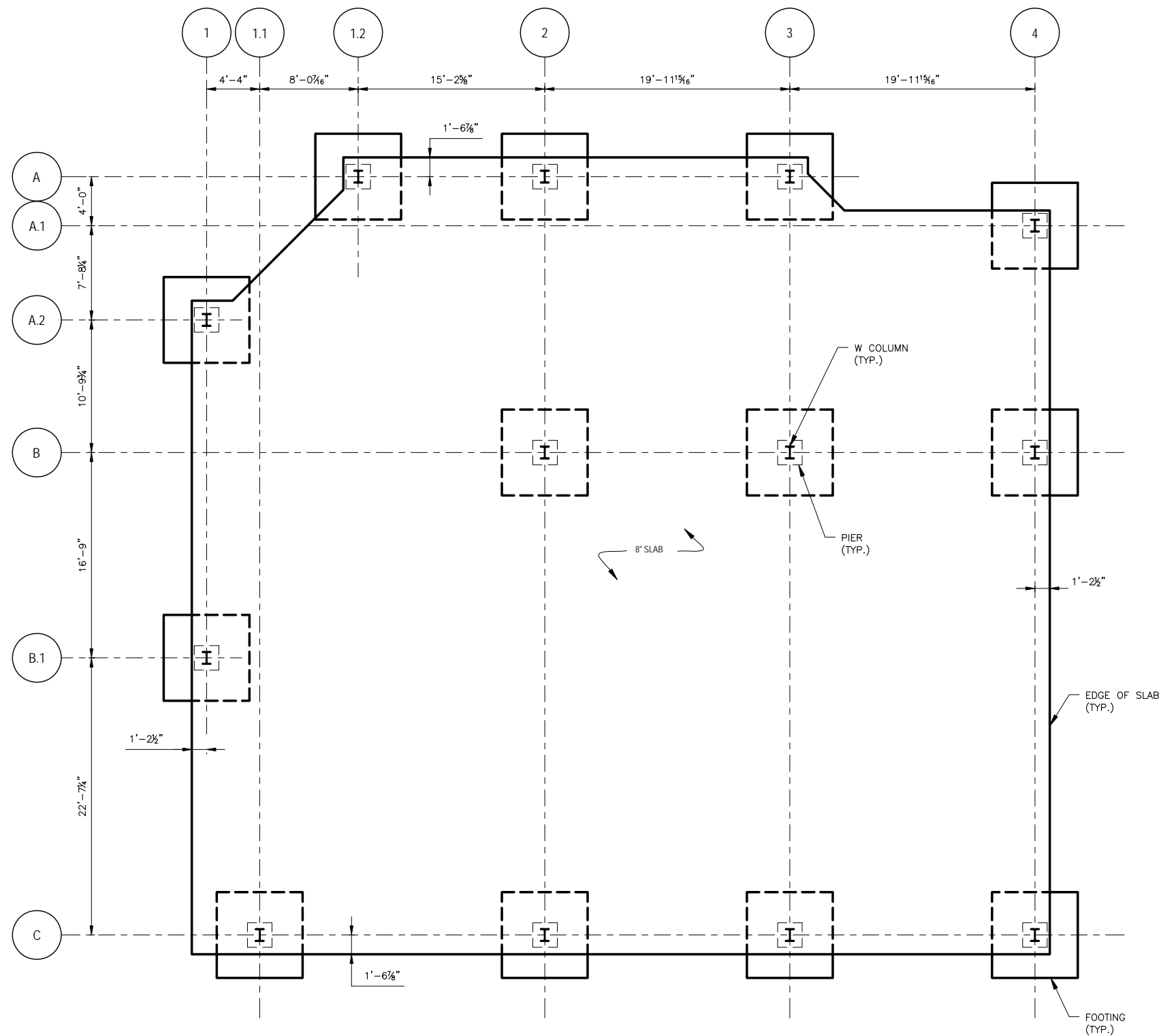
H.NI	Y.VAYNER	J.CHU
Designed by	Drawn by	Checked by

Date 07/15/2013

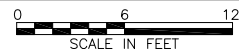
Contract Number LGA - 124.223

Drawing Number S003

PID# 12801327

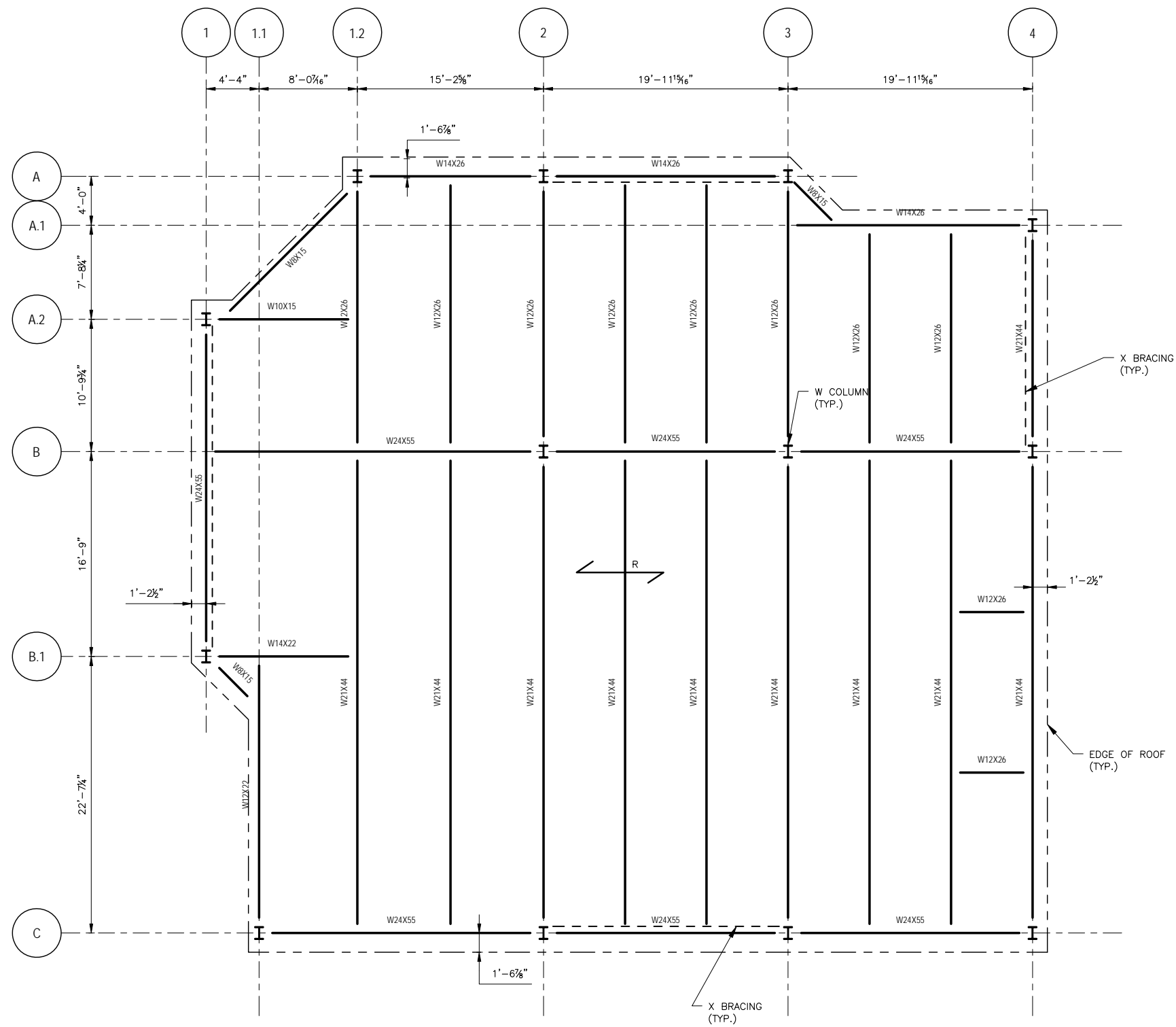


GROUND FLOOR PLAN



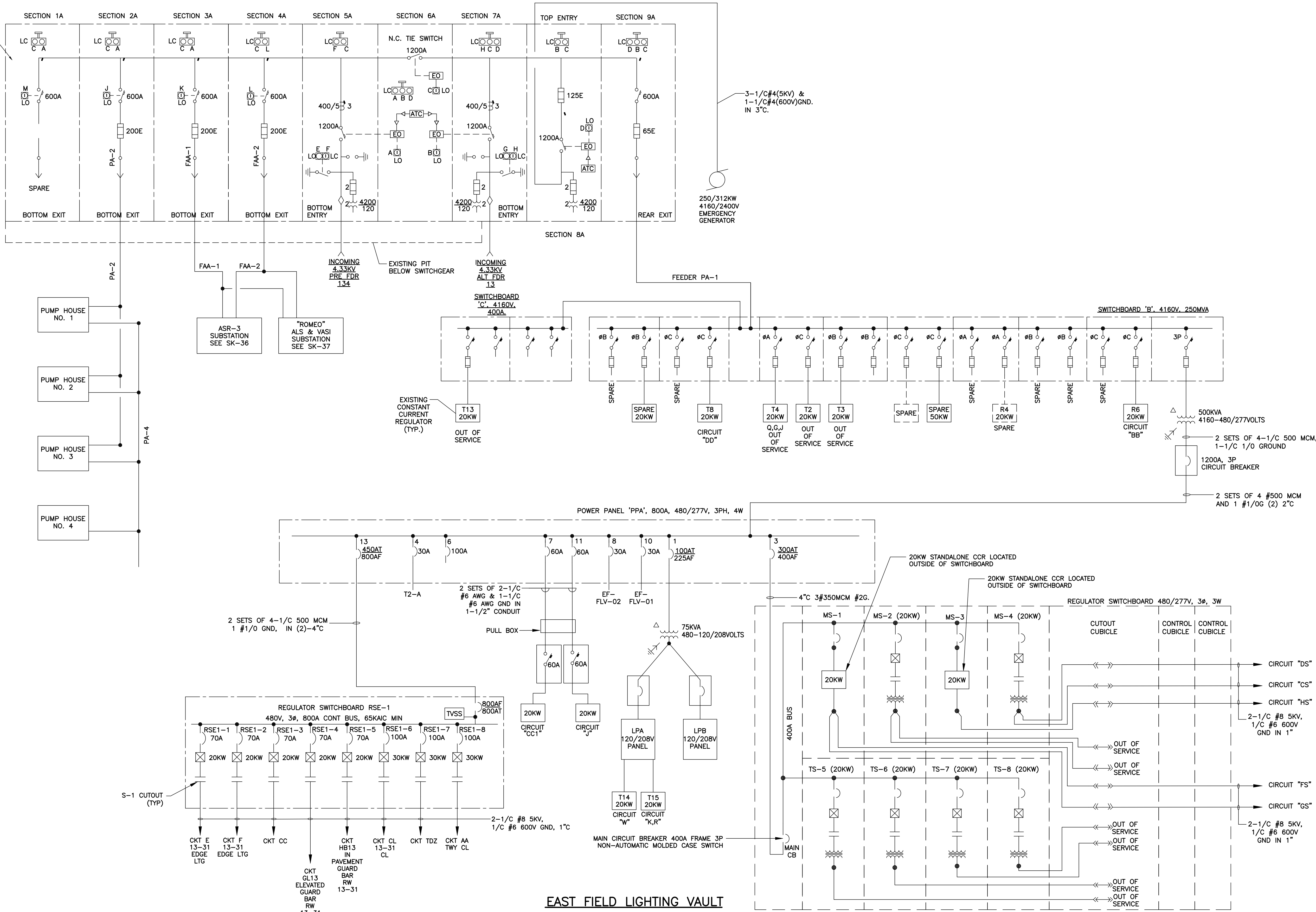
30% SUBMISSION
07/15/2013

No.	Date	Revision	Approved
ENGINEERING DEPARTMENT			
LAGUARDIA AIRPORT			
STRUCTURAL			
Title			
REPLACEMENT OF THE EAST FIELD LIGHTING VAULT			
ROOF FRAMING PLAN			
<small>This drawing subject to conditions in contract. All inventions, ideas, designs and methods herein are reserved to Port Authority and may not be used without its written consent. All recipients of Contract documents, including bidders and those who do not bid and their prospective subcontractors and suppliers who may receive all or a part of the Contract documents or copies thereof, shall make every effort to ensure the secure and appropriate disposal of the Contract documents to prevent further disclosure of the information contained in the documents. Secure and appropriate disposal includes methods of document destruction such as shredding or arrangements with refuse handlers that ensure that third persons will not have access to the documents' contents either before, during, or after disposal. Documents may also be returned for disposal purposes to the Contract Desk on the 3rd Floor, 3 Gateway Center, Newark NJ 07102 or the office of the Director of Documents, Two Montgomery Street, 3rd Floor, Jersey City, NJ 07302. It is a violation of law for any person to alter a document in any way, unless acting under the direction of a licensed professional engineer or registered architect. If this document bearing the seal of an engineer/architect is altered, the altering engineer/architect shall affix to the document their seal and the notation "altered by" followed by their signature and the date of such alteration, and a specific description of the alteration.</small>			
H.NI	Y.VAYNER	J.CHU	
Designed by	Drawn by	Checked by	
Date	07/15/2013		
Contract Number	LGA - 124.223		
Drawing Number	S004		
	PID# 12801327		



ROOF FRAMING PLAN
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270MVA
SEPT 2004



Sheet of

**THE PORT AUTHORITY
OF NY & NJ**

CHIEF ELECTRICAL ENGINEER

30% SUBMISSION
07/15/2013

No.	Date	Revision	Approved

ENGINEERING DEPARTMENT

**LAGUARDIA
AIRPORT**

ELECTRICAL

Title
REPLACEMENT OF THE EAST FIELD
LIGHTING VAULT

**EXISTING EFLV
ONE LINE DIAGRAM**

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G. GADOMSKI G. GADOMSKI E. FAYMAN
Designed by Drawn by Checked by

Date 07/15/2013
WORK ORDER No. # X (TBD)
Contract Number LGA - 124.223

Drawing Number **E100**
PID# 12801327

CHIEF ELECTRICAL ENGINEER

30% SUBMISSION
 07/15/2013

No.	Date	Revision	Approved

ENGINEERING DEPARTMENT			
LAGUARDIA AIRPORT			

ELECTRICAL

Title
REPLACEMENT OF THE EAST FIELD LIGHTING VAULT

EFLV ONE LINE DIAGRAM

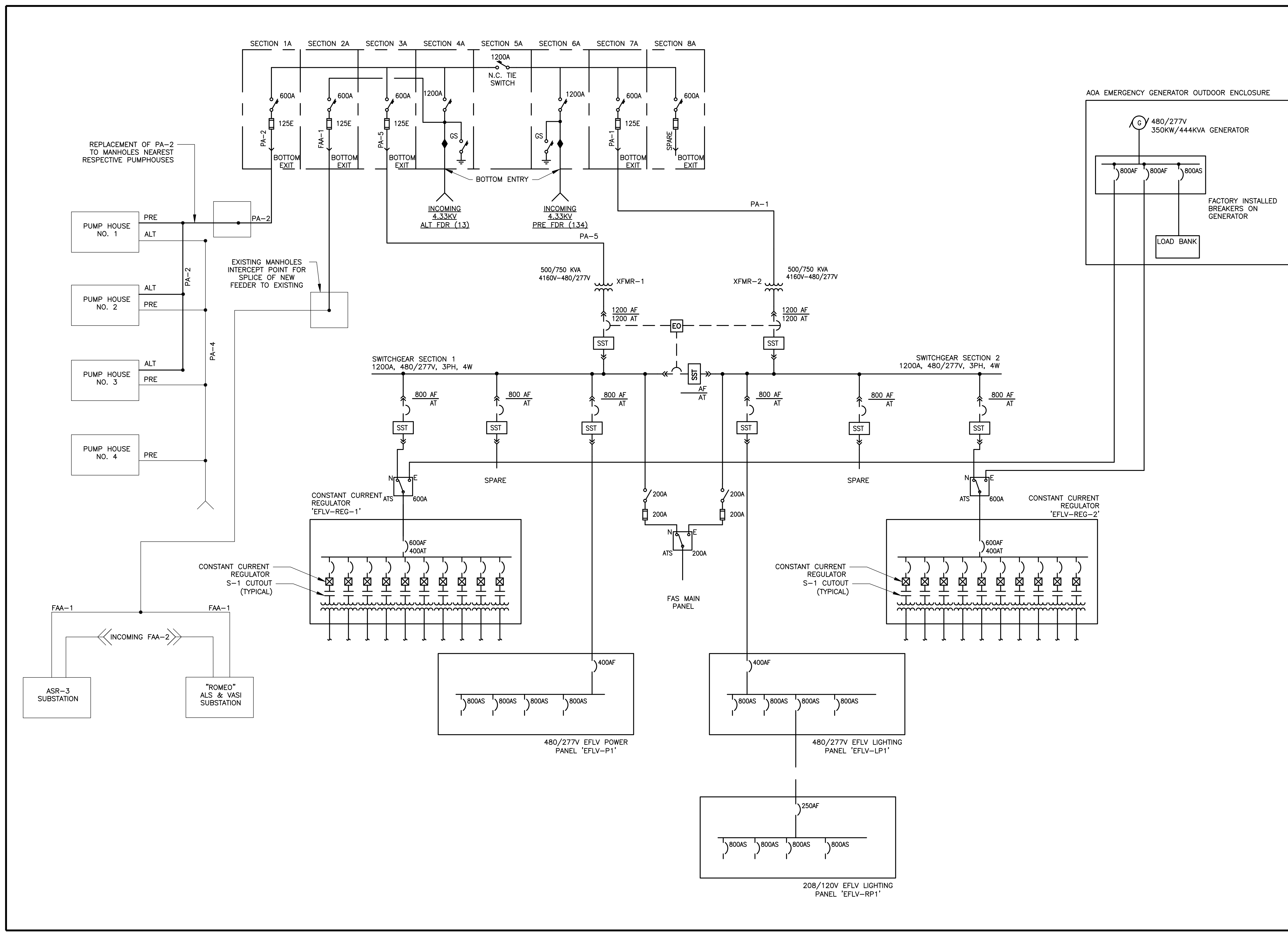
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G. GADOMSKI **G. GADOMSKI** **E. FAYMAN**
 Designed by Drawn by Checked by

Date **07/15/2013**

WORK ORDER No. # X (TBD)
 Contract Number **LGA - 124.223**

Drawing Number **E101**
 PID# 12801327



CHIEF ELECTRICAL ENGINEER

30% SUBMISSION
07/15/2013

No.	Date	Revision	Approved

ENGINEERING DEPARTMENT

LAGUARDIA
AIRPORT

ELECTRICAL

Title
REPLACEMENT OF THE EAST FIELD
LIGHTING VAULT

REMOVAL PLAN
-1-

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M. TATARA M. TATARA E. FAYMAN
Designed by Drawn by Checked by

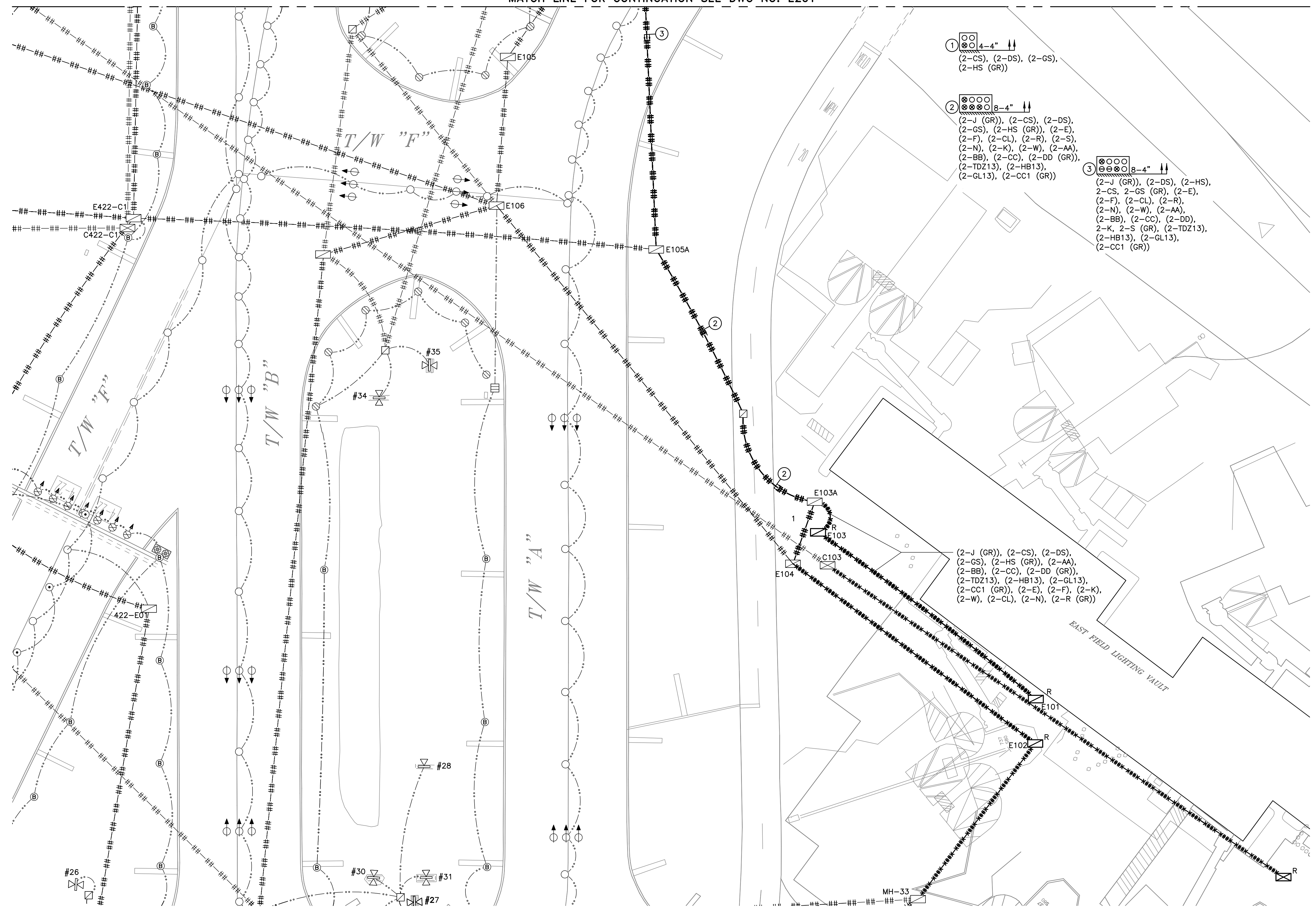
Date 07/15/2013

WORK ORDER No. # X (TBD)
Contract LGA - 124.223
Number

Drawing E200
Number

PID# 12801327

MATCH LINE FOR CONTINUATION SEE DWG NO. E201



1
4-4" ↑↑
(2-CS), (2-DS), (2-GS),
(2-HS (GR))

2
8-4" ↑↑
(2-J (GR)), (2-CS), (2-DS),
(2-GS), (2-HS (GR)), (2-E),
(2-F), (2-CL), (2-R), (2-S),
(2-N), (2-K), (2-W), (2-AA),
(2-BB), (2-CC), (2-DD (GR)),
(2-TDZ13), (2-HB13),
(2-GL13), (2-CC1 (GR))

3
8-4" ↑↑
(2-J (GR)), (2-DS), (2-HS),
(2-CS, 2-GS (GR), (2-E),
(2-F), (2-CL), (2-R),
(2-N), (2-W), (2-AA),
(2-BB), (2-CC), (2-DD),
(2-K, 2-S (GR), (2-TDZ13),
(2-HB13), (2-GL13),
(2-CC1 (GR))

(2-J (GR)), (2-CS), (2-DS),
(2-GS), (2-HS (GR)), (2-AA),
(2-BB), (2-CC), (2-DD (GR)),
(2-TDZ13), (2-HB13), (2-GL13),
(2-CC1 (GR)), (2-E), (2-F), (2-K),
(2-W), (2-CL), (2-N), (2-R (GR))

CHIEF ELECTRICAL ENGINEER

30% SUBMISSION
07/15/2013

No.	Date	Revision	Approved

ENGINEERING DEPARTMENT

LAGUARDIA
AIRPORT

ELECTRICAL

Title
REPLACEMENT OF THE EAST FIELD
LIGHTING VAULT

REMOVAL PLAN
-2-

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M. TATARA M. TATARA E. FAYFMAN
Designed by Drawn by Checked by

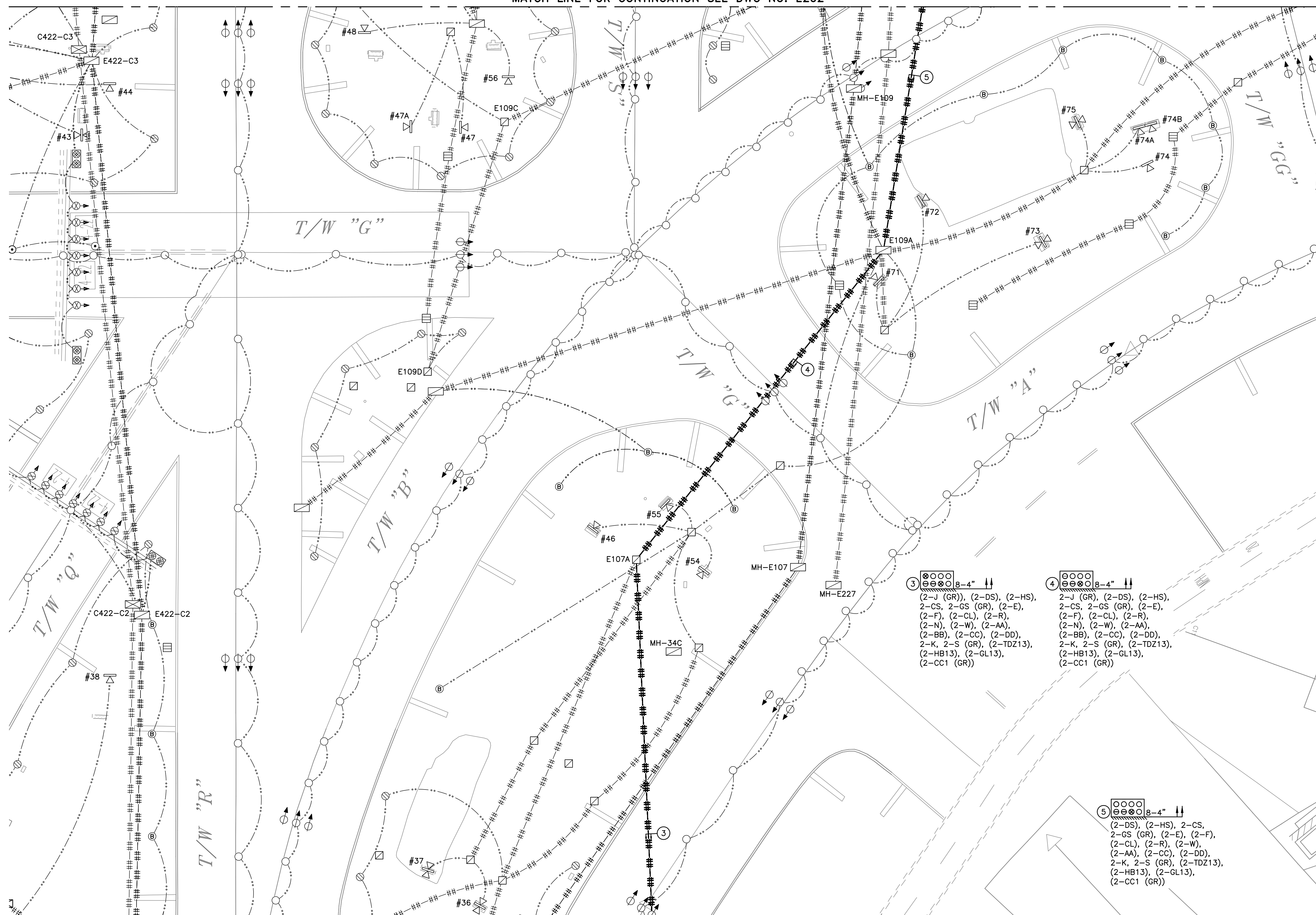
Date 07/15/2013

WORK ORDER No. # X (TBD)
Contract LGA - 124.223
Number

Drawing E201
Number

PID# 12801327

MATCH LINE FOR CONTINUATION SEE DWG NO. E202



30% SUBMISSION
07/15/2013

No.	Date	Revision	Approved

ENGINEERING DEPARTMENT

LAGUARDIA
AIRPORT

ELECTRICAL

Title
REPLACEMENT OF THE EAST FIELD
LIGHTING VAULT

REMOVAL PLAN
-3-

This drawing subject to conditions in contract. All inventions, ideas, designs and methods herein are reserved to Port Authority and may not be used without its written consent. All recipients of Contract documents, including bidders and those who do not bid and their prospective subcontractors and suppliers who may receive all or a part of the Contract documents or copies thereof, shall make every effort to ensure the secure and appropriate disposal of the Contract documents to prevent further disclosure of the information contained in the documents. Secure and appropriate disposal includes methods of document destruction such as shredding or arrangements with refuse handlers that ensure that third persons will not have access to the documents' contents either before, during, or after disposal. Documents may also be returned for disposal purposes to the Contract Desk on the 3rd Floor, 3 Gateway Center, Newark, NJ 07102 or the office of the Director of Procurement, Two Montgomery Street, 3rd Floor, Jersey City, NJ 07302.

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M. TATARA	M. TATARA	E. FAYMAN
Designed by	Drawn by	Checked by

Date 07/15/2013

WORK ORDER No. # X (TBD)
Contract Number LGA - 124.223

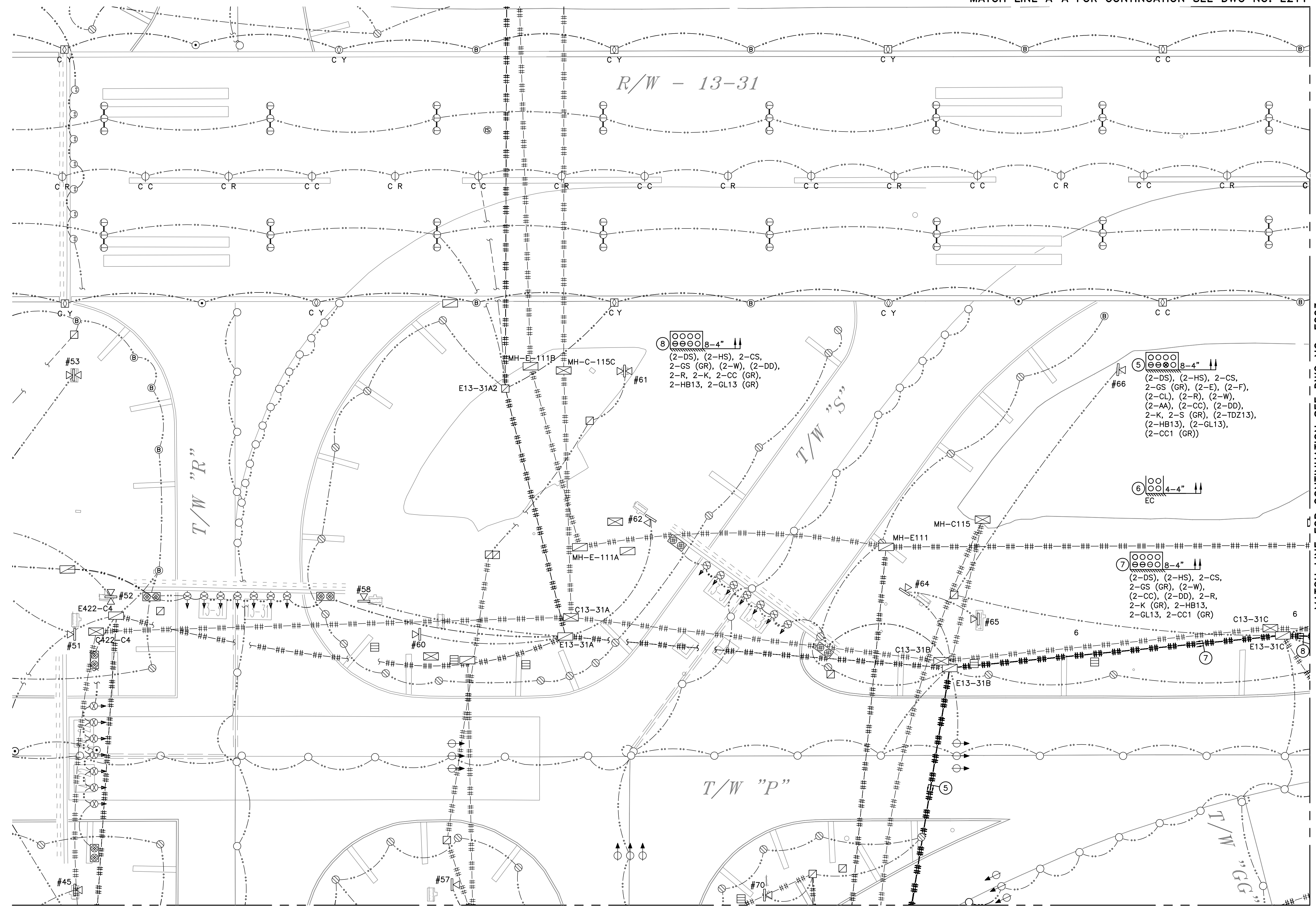
Drawing Number **E202**
PID# 12801327

MATCH LINE A-A FOR CONTINUATION SEE DWG NO. E211

R/W - 13-31

MATCH LINE FOR CONTINUATION SEE DWG NO. E203

MATCH LINE FOR CONTINUATION SEE DWG NO. E201



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07/15/2013

No.	Date	Revision	Approved

ENGINEERING DEPARTMENT

LAGUARDIA
AIRPORT

ELECTRICAL

Title
REPLACEMENT OF THE EAST FIELD
LIGHTING VAULT

REMOVAL PLAN
-4-

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M. TATAR M. TATAR E. FAYFAN
Designed by Drawn by Checked by

Date 07/15/2013

WORK ORDER No. # X (TBD)
Contract Number LGA - 124.223

Drawing Number E203
PID# 12801327

MATCH LINE A-A FOR CONTINUATION SEE DWG NO. E211

R/W - 13-31

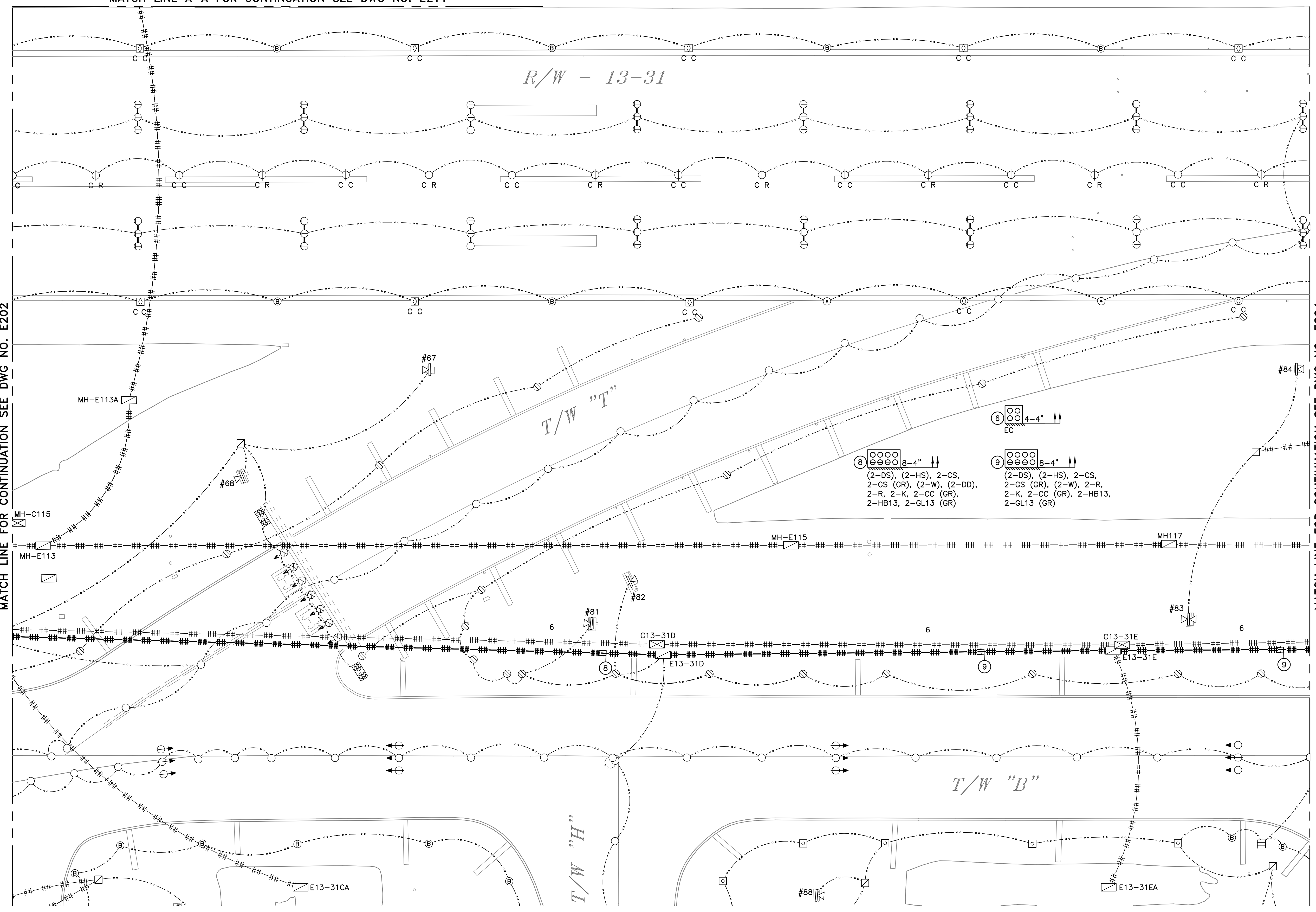
T/W "T"

T/W "B"

T/W "H"

MATCH LINE FOR CONTINUATION SEE DWG NO. E202

MATCH LINE FOR CONTINUATION SEE DWG NO. E204



30% SUBMISSION
07/15/2013

No.	Date	Revision	Approved

ENGINEERING DEPARTMENT			
LAGUARDIA AIRPORT			

ELECTRICAL

Title
**REPLACEMENT OF THE EAST FIELD
LIGHTING VAULT**

**REMOVAL PLAN
-5-**

This drawing subject to conditions in contract. All inventions, ideas, designs and methods herein are reserved to Port Authority and may not be used without its written consent. All recipients of Contract documents, including bidders and those who do not bid and their prospective subcontractors and suppliers who may receive all or a part of the Contract documents or copies thereof, shall make every effort to ensure the secure and appropriate disposal of the Contract documents to prevent further disclosure of the information contained in the documents. Secure and appropriate disposal includes methods of document destruction such as shredding or arrangements with refuse handlers that ensure that third persons will not have access to the documents' contents either before, during, or after disposal. Documents may also be returned for disposal purposes to the Contract Desk on the 3rd Floor, 3 Gateway Center, Newark, NJ 07102 or the office of the Director of Procurement, Two Montgomery Street, 3rd Floor, Jersey City, NJ 07302. It is a violation of law for any person to alter a document in any way, unless acting under the direction of a licensed professional engineer or registered architect. If this document, bearing the seal of an engineer/architect is altered, the altering engineer/architect shall affix to the document their seal and the notation "altered by" followed by their signature and the date of such alteration, and a specific description of the alteration.

M. TATARA M. TATARA E. FAYMAN
Designed by Drawn by Checked by

Date 07/15/2013
WORK ORDER No. # X (TBD)
Contract
Number **LGA - 124.223**

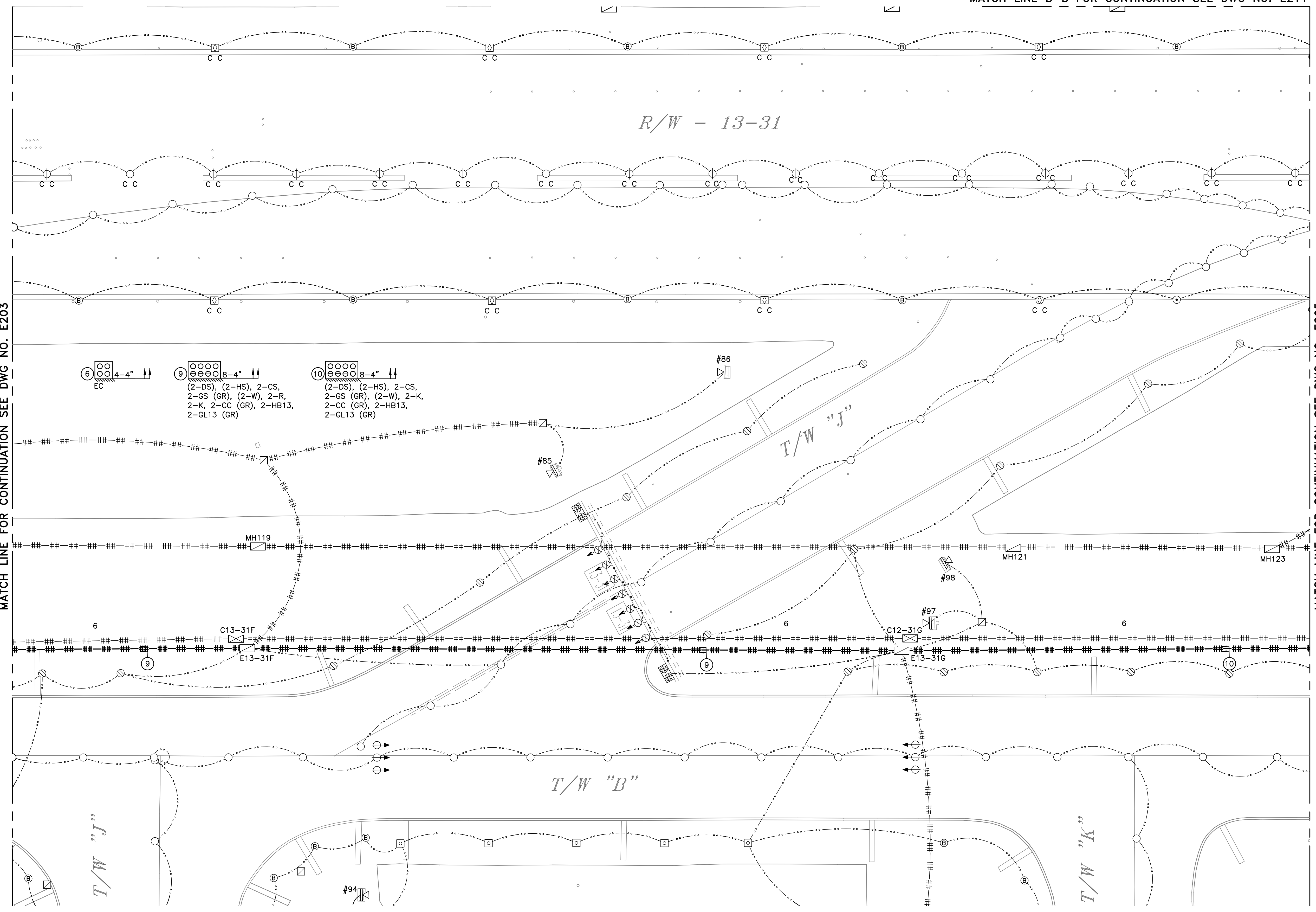
Drawing
Number **E204**
PID# 12801327

MATCH LINE B-B FOR CONTINUATION SEE DWG NO. E211

R/W - 13-31

MATCH LINE FOR CONTINUATION SEE DWG NO. E203

MATCH LINE FOR CONTINUATION SEE DWG NO. E205



- 6 4-4" EC
- 9 8-4" EC
(2-DS), (2-HS), 2-CS,
2-GS (GR), (2-W), 2-R,
2-K, 2-CC (GR), 2-HB13,
2-GL13 (GR)
- 10 8-4" EC
(2-DS), (2-HS), 2-CS,
2-GS (GR), (2-W), 2-K,
2-CC (GR), 2-HB13,
2-GL13 (GR)

30% SUBMISSION
07/15/2013

No.	Date	Revision	Approved

ENGINEERING DEPARTMENT			
LAGUARDIA AIRPORT			

ELECTRICAL

Title
**REPLACEMENT OF THE EAST FIELD
LIGHTING VAULT**

**REMOVAL PLAN
-6-**

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M. TATARA **M. TATARA** **E. FAYMAN**
Designed by Drawn by Checked by

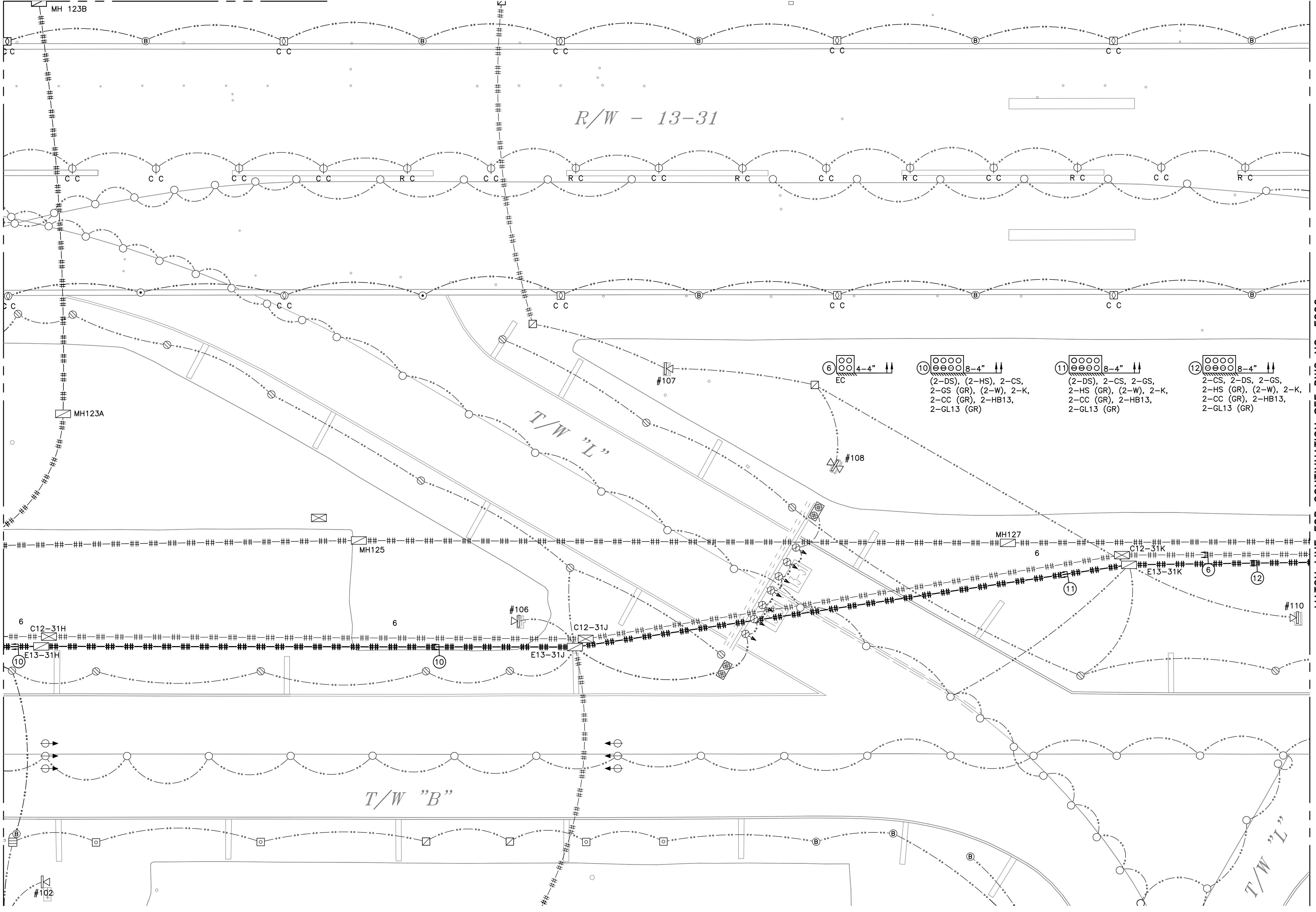
Date 07/15/2013
WORK ORDER No. # X (TBD)
Contract Number **LGA - 124.223**

Drawing Number **E205**
PID# 12801327

MATCH LINE B-B FOR CONTINUATION SEE DWG NO. E211

MATCH LINE FOR CONTINUATION SEE DWG NO. E204

MATCH LINE FOR CONTINUATION SEE DWG NO. E206



R/W - 13-31

T/W "L"

T/W "B"

T/W "L"

CHIEF ELECTRICAL ENGINEER

30% SUBMISSION
07/15/2013

No.	Date	Revision	Approved

ENGINEERING DEPARTMENT			
LAGUARDIA AIRPORT			

ELECTRICAL

Title
REPLACEMENT OF THE EAST FIELD LIGHTING VAULT

**REMOVAL PLAN
-7-**

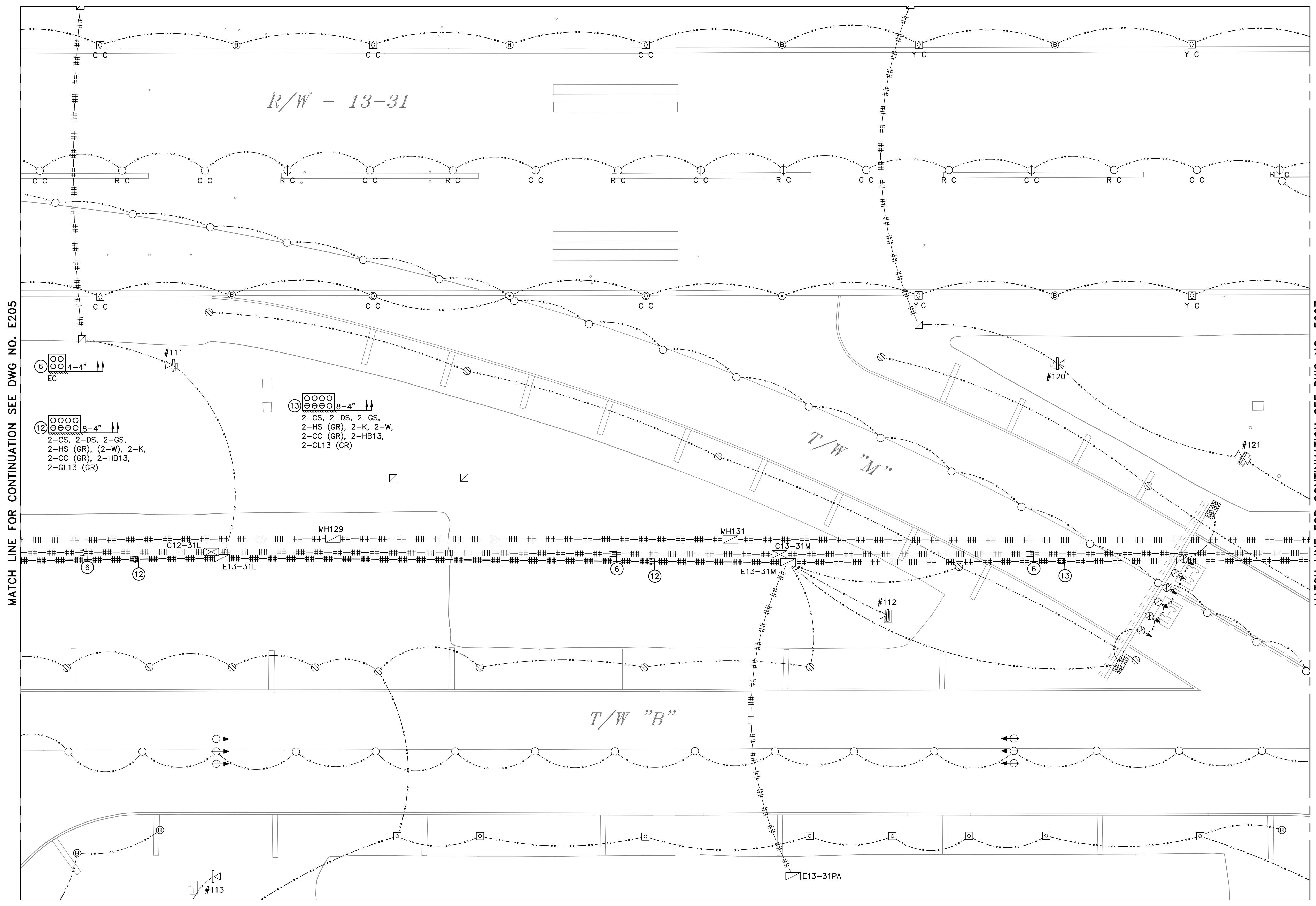
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M. TATAR M. TATAR E. FAYFAN
Designed by Drawn by Checked by

Date 07/15/2013

WORK ORDER No. # X (TBD)
Contract Number LGA - 124.223

Drawing Number **E206**
PID# 12801327



MATCH LINE FOR CONTINUATION SEE DWG NO. E205

MATCH LINE FOR CONTINUATION SEE DWG NO. E207

R/W - 13-31

T/W "M"

T/W "B"

⑥ 4-4" EC

⑫ 8-4" 2-CS, 2-DS, 2-GS, 2-HS (GR), (2-W), 2-K, 2-CC (GR), 2-HB13, 2-GL13 (GR)

⑬ 8-4" 2-CS, 2-DS, 2-GS, 2-HS (GR), 2-K, 2-W, 2-CC (GR), 2-HB13, 2-GL13 (GR)

⑫

⑫

⑫

⑬

#113

E13-31PA

#111

#120

#121

#112

MH129

MH131

C12-31L

E13-31L

E13-31M

C13-31M

30% SUBMISSION
07/15/2013

No.	Date	Revision	Approved

ENGINEERING DEPARTMENT

LAGUARDIA
AIRPORT

ELECTRICAL

Title
REPLACEMENT OF THE EAST FIELD
LIGHTING VAULT

REMOVAL PLAN
-8-

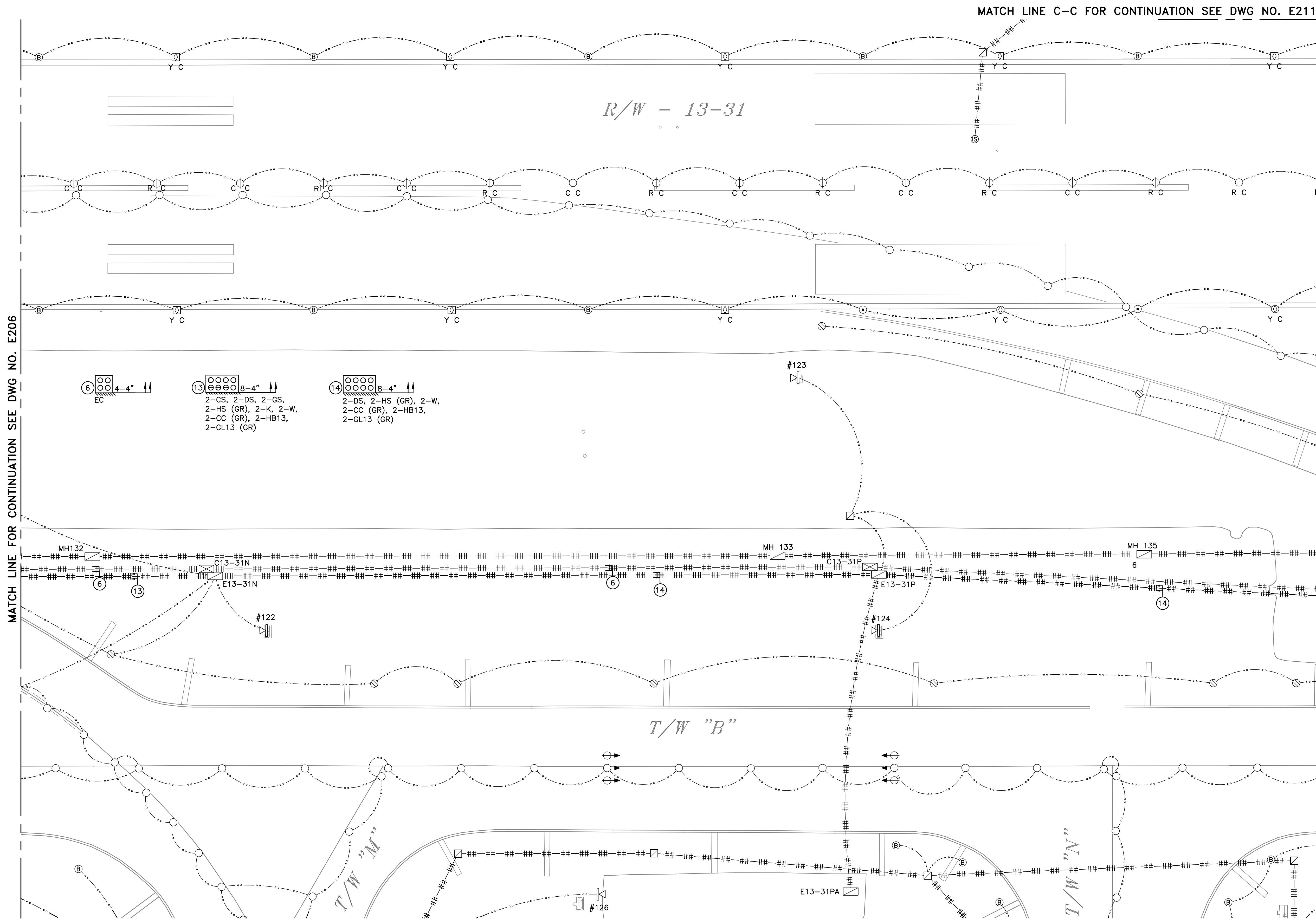
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M. TATARA M. TATARA E. FAYMAN
Designed by Drawn by Checked by

Date 07/15/2013

WORK ORDER No. # X (TBD)
Contract Number LGA - 124.223

Drawing Number E207
PID# 12801327



MATCH LINE FOR CONTINUATION SEE DWG NO. E206

MATCH LINE FOR CONTINUATION SEE DWG NO. E208

R/W - 13-31

T/W "B"

MATCH LINE C-C FOR CONTINUATION SEE DWG NO. E211

CHIEF ELECTRICAL ENGINEER

30% SUBMISSION
07/15/2013

No.	Date	Revision	Approved

ENGINEERING DEPARTMENT			
LAGUARDIA AIRPORT			

ELECTRICAL

Title
**REPLACEMENT OF THE EAST FIELD
LIGHTING VAULT**

**REMOVAL PLAN
-9-**

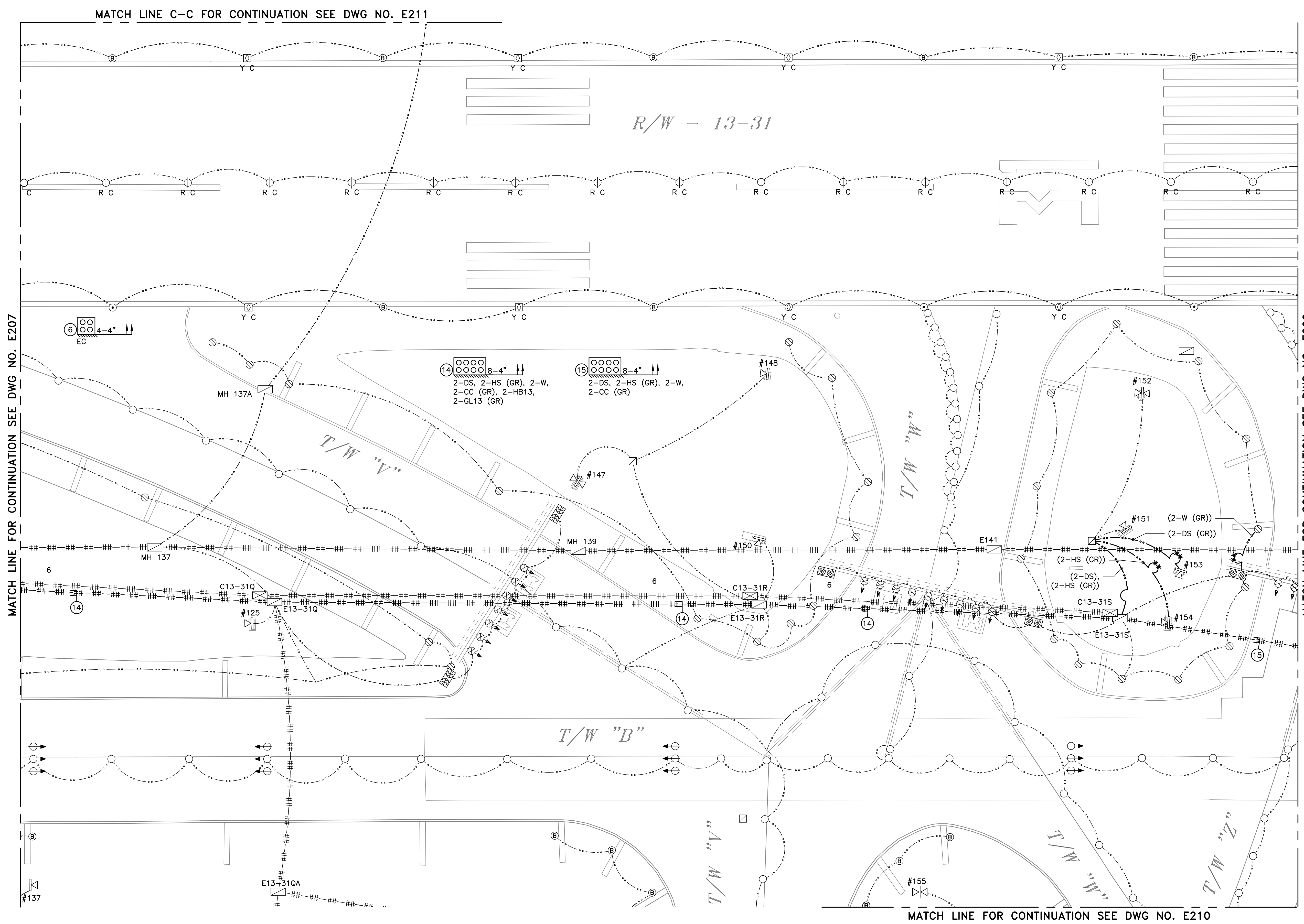
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M. TATARA M. TATARA E. FAYFMAN
Designed by Drawn by Checked by

Date 07/15/2013

WORK ORDER No. # X (TBD)
Contract Number **LGA - 124.223**

Drawing Number **E208**
PID# 12801327



30% SUBMISSION
07/15/2013

No.	Date	Revision	Approved

ENGINEERING DEPARTMENT

LAGUARDIA
AIRPORT

ELECTRICAL

Title
REPLACEMENT OF THE EAST FIELD
LIGHTING VAULT

REMOVAL PLAN
- 10 -

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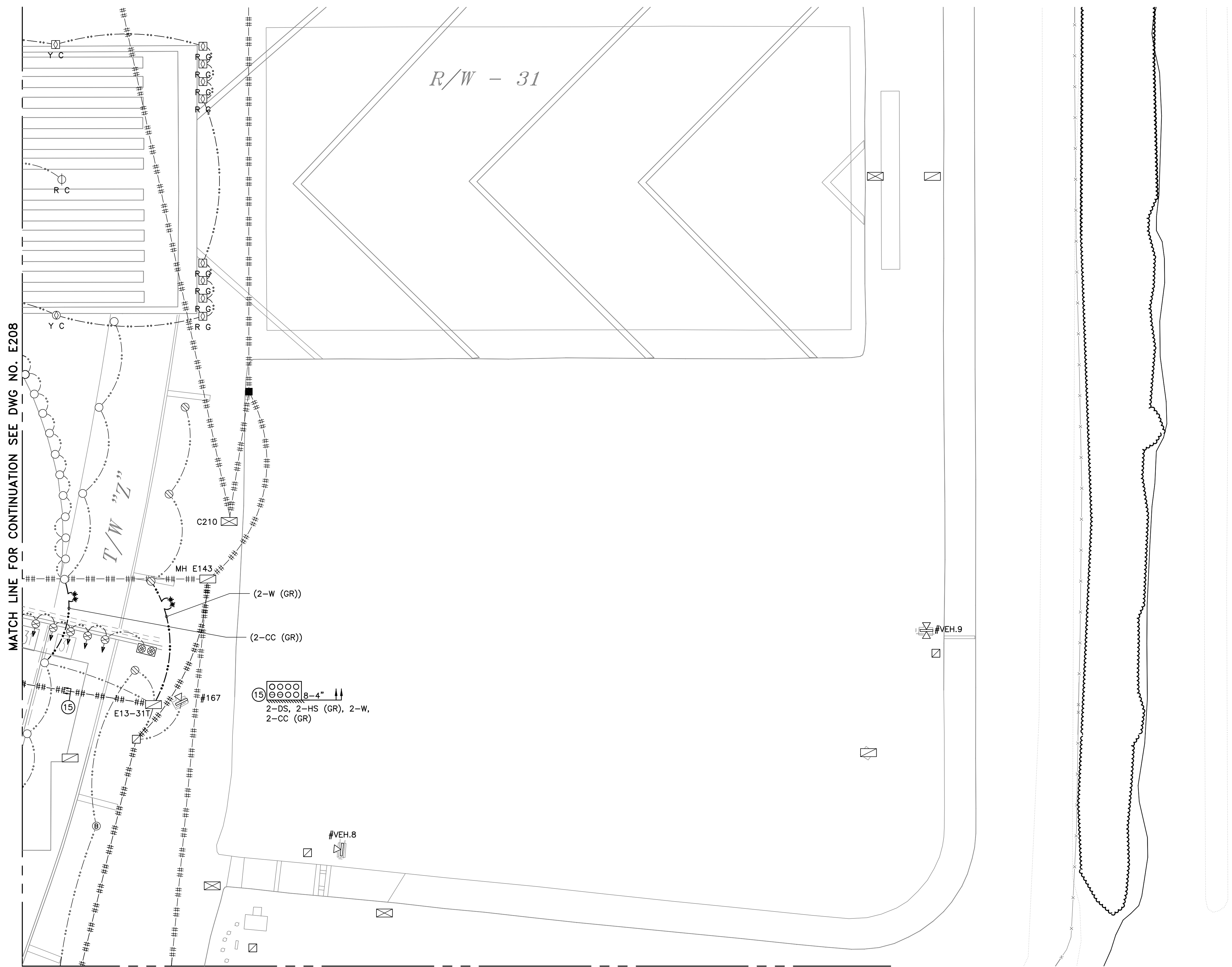
M. TATARA M. TATARA E. FAYMAN
Designed by Drawn by Checked by

Date 07/15/2013

WORK ORDER No. # X (TBD)
Contract Number LGA - 124.223

Drawing Number E209

PID# 12801327



MATCH LINE FOR CONTINUATION SEE DWG NO. E208

MATCH LINE FOR CONTINUATION SEE DWG NO. E210

15
8-4" ↑↑
2-DS, 2-HS (GR), 2-W,
2-CC (GR)

**THE PORT AUTHORITY
OF NY & NJ**

CHIEF ELECTRICAL ENGINEER

30% SUBMISSION
07/15/2013

No.	Date	Revision	Approved

ENGINEERING DEPARTMENT

LAGUARDIA
AIRPORT

ELECTRICAL

Title
REPLACEMENT OF THE EAST FIELD
LIGHTING VAULT

REMOVAL PLAN
- 11 -

This drawing subject to conditions in contract. All inventions, ideas, designs and methods herein are reserved to Port Authority and may not be used without its written consent. All recipients of Contract documents, including bidders and those who do not bid and their prospective subcontractors and suppliers who may receive all or a part of the Contract documents or copies thereof, shall make every effort to ensure the secure and appropriate disposal of the Contract documents to prevent further disclosure of the information contained in the documents. Secure and appropriate disposal includes methods of document destruction such as shredding or arrangements with refuse handlers that ensure that third persons will not have access to the documents' contents either before, during, or after disposal. Documents may also be returned for disposal purposes to the Contract Desk on the 3rd Floor, 3 Gateway Center, Newark, NJ 07102 or the office of the Director of Procurement, Two Montgomery Street, 3rd Floor, Jersey City, NJ 07302. It is a violation of law for any person to alter a document in any way, unless acting under the direction of a licensed professional engineer or registered architect. If this document, bearing the seal of an engineer/architect is altered, the altering engineer/architect shall affix to the document their seal and the notation "altered by" followed by their signature and the date of such alteration, and a specific description of the alteration.

M. TATARA M. TATARA E. FAYFMAN
Designed by Drawn by Checked by

Date 07/15/2013

WORK ORDER No. # X (TBD)
Contract LGA - 124.223
Number

Drawing E210
Number PID# 12801327

MATCH LINE FOR CONTINUATION SEE DWG NO. E208

MATCH LINE FOR CONTINUATION SEE DWG NO. E209



16
12-4" ↑↑
FDR #12, P501, P801,
FDR #23, P401, P701,
PA-4, P301, P601

**THE PORT AUTHORITY
OF NY & NJ**

CHIEF ELECTRICAL ENGINEER

30% SUBMISSION
07/15/2013

No.	Date	Revision	Approved

ENGINEERING DEPARTMENT

LAGUARDIA
AIRPORT

ELECTRICAL

Title
REPLACEMENT OF THE EAST FIELD
LIGHTING VAULT

PARTIAL REMOVAL PLAN
- 1 -

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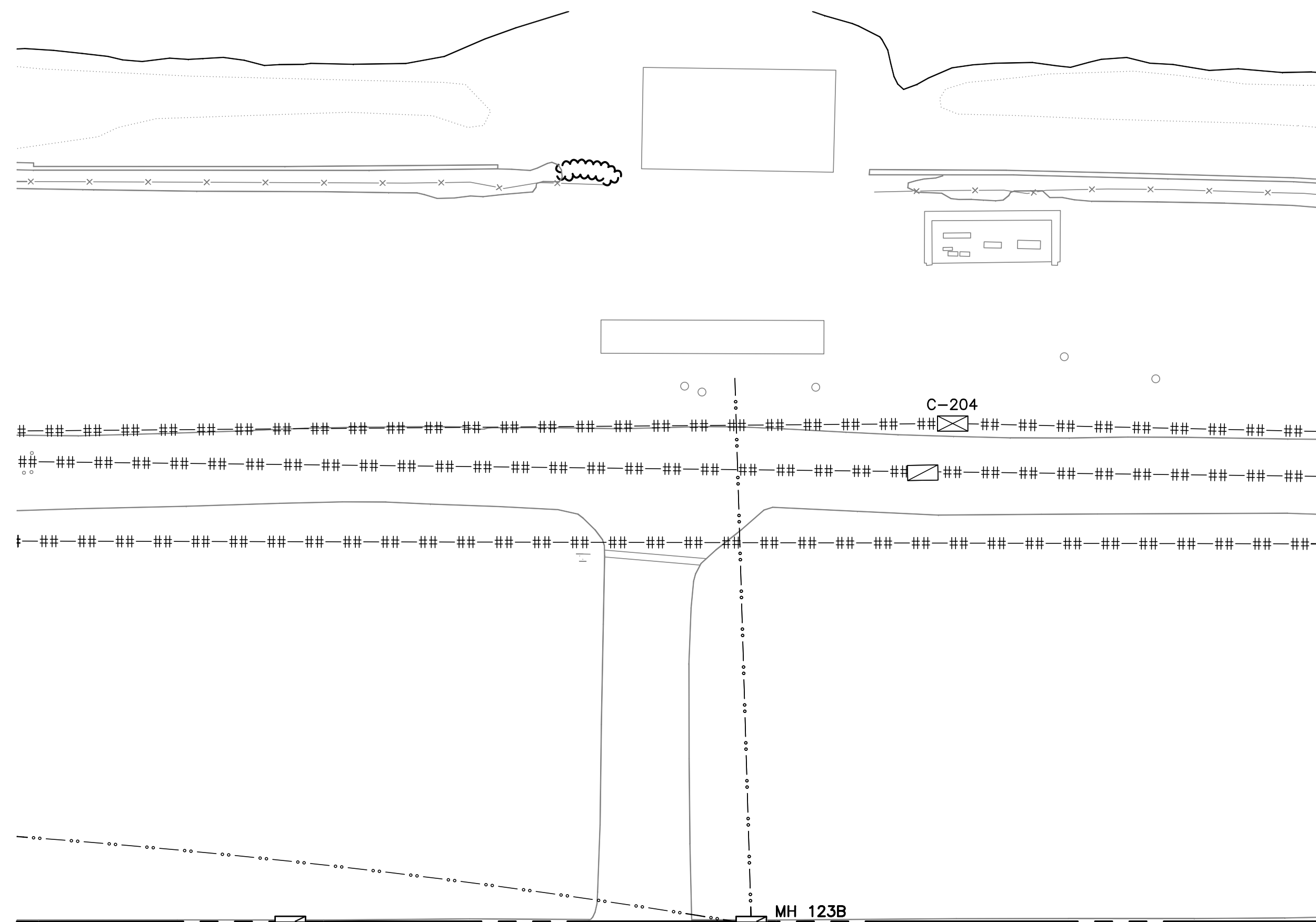
M. TATARA M. TATARA E. FAYMAN
Designed by Drawn by Checked by

Date 07/15/2013

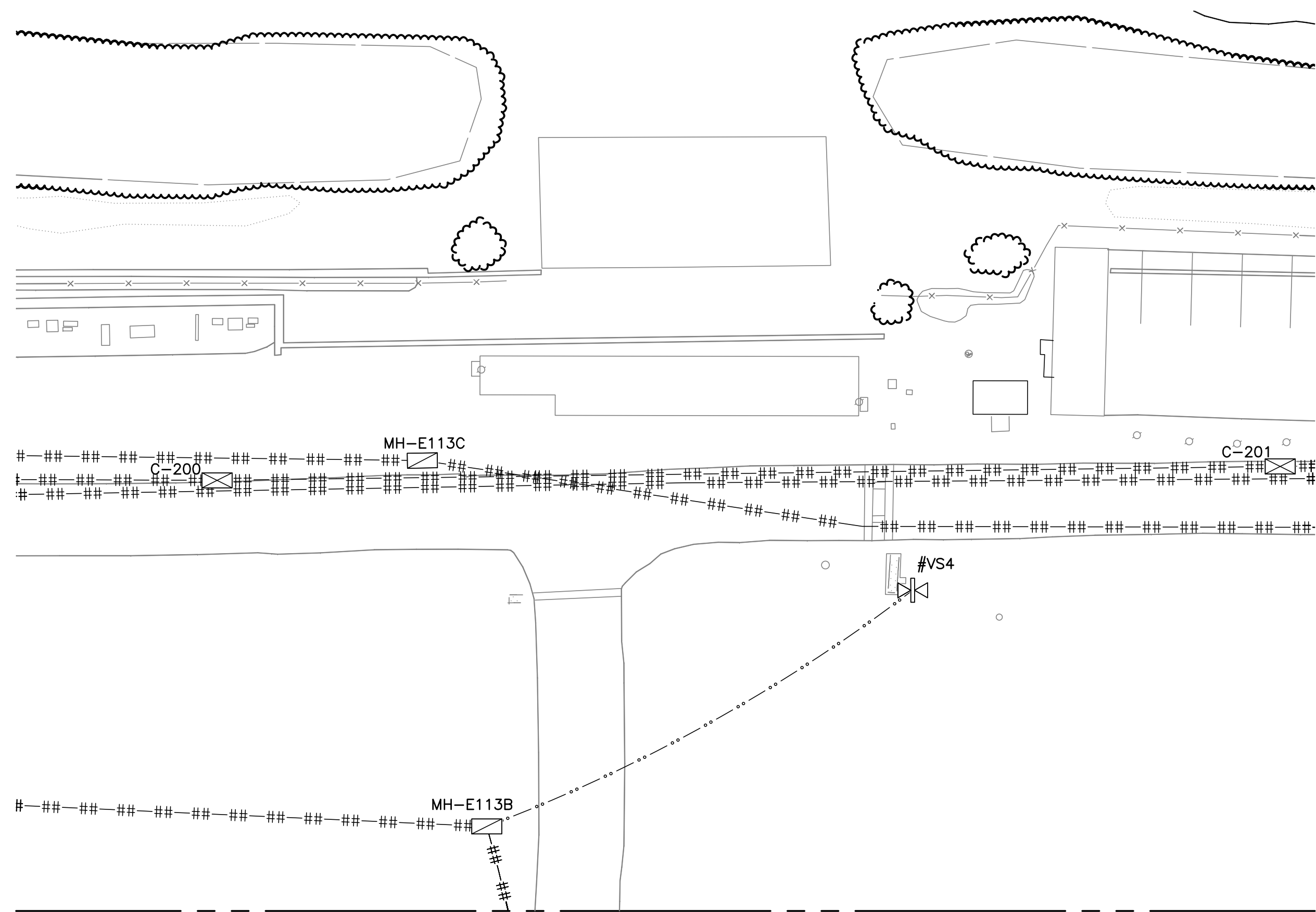
WORK ORDER No. # X (TBD)
Contract Number LGA - 124.223

Drawing Number E211

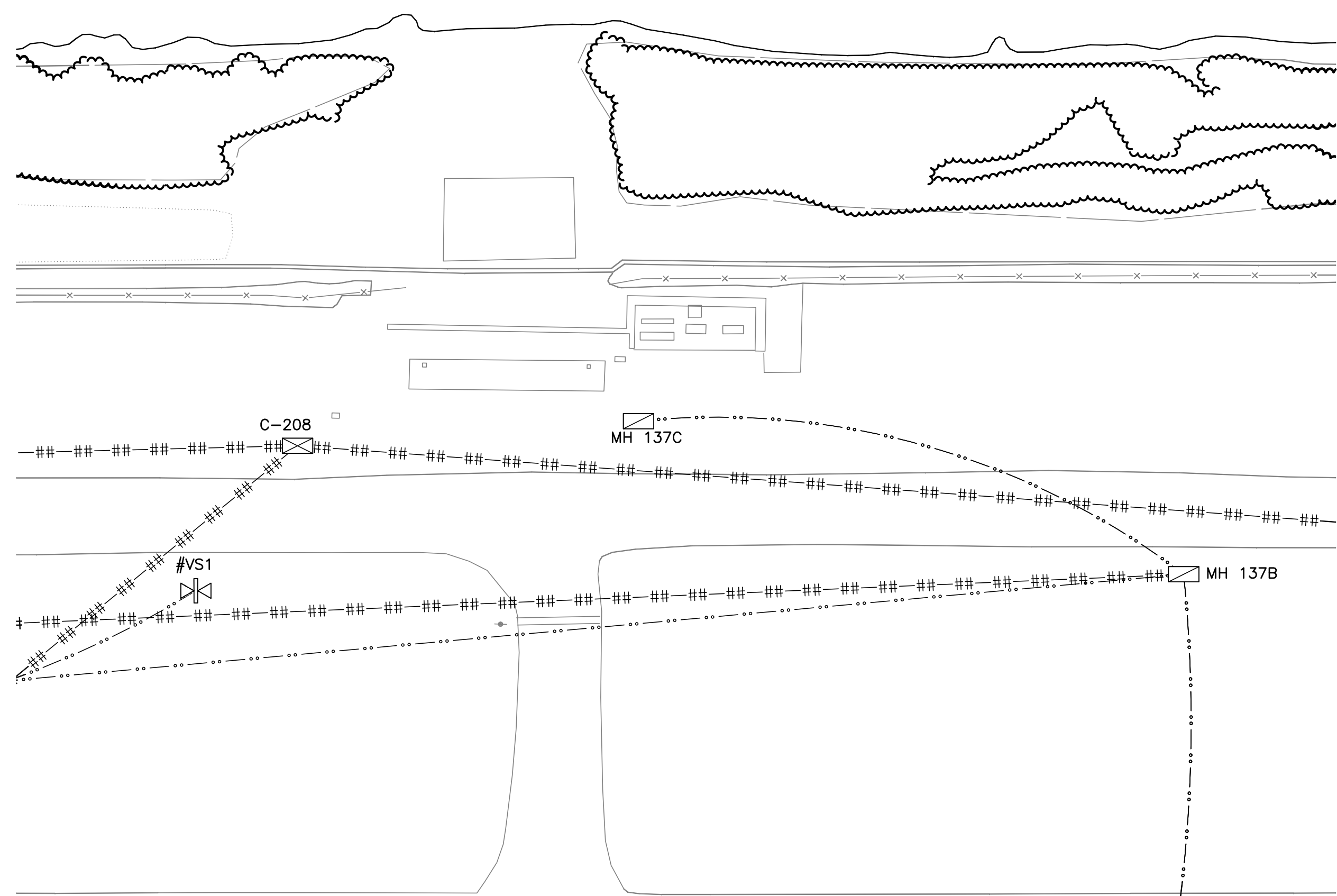
PID# 12801327



FOR CONTINUATION SEE DWG NO. E204 MATCH LINE B-B FOR CONTINUATION SEE DWG NO. E205

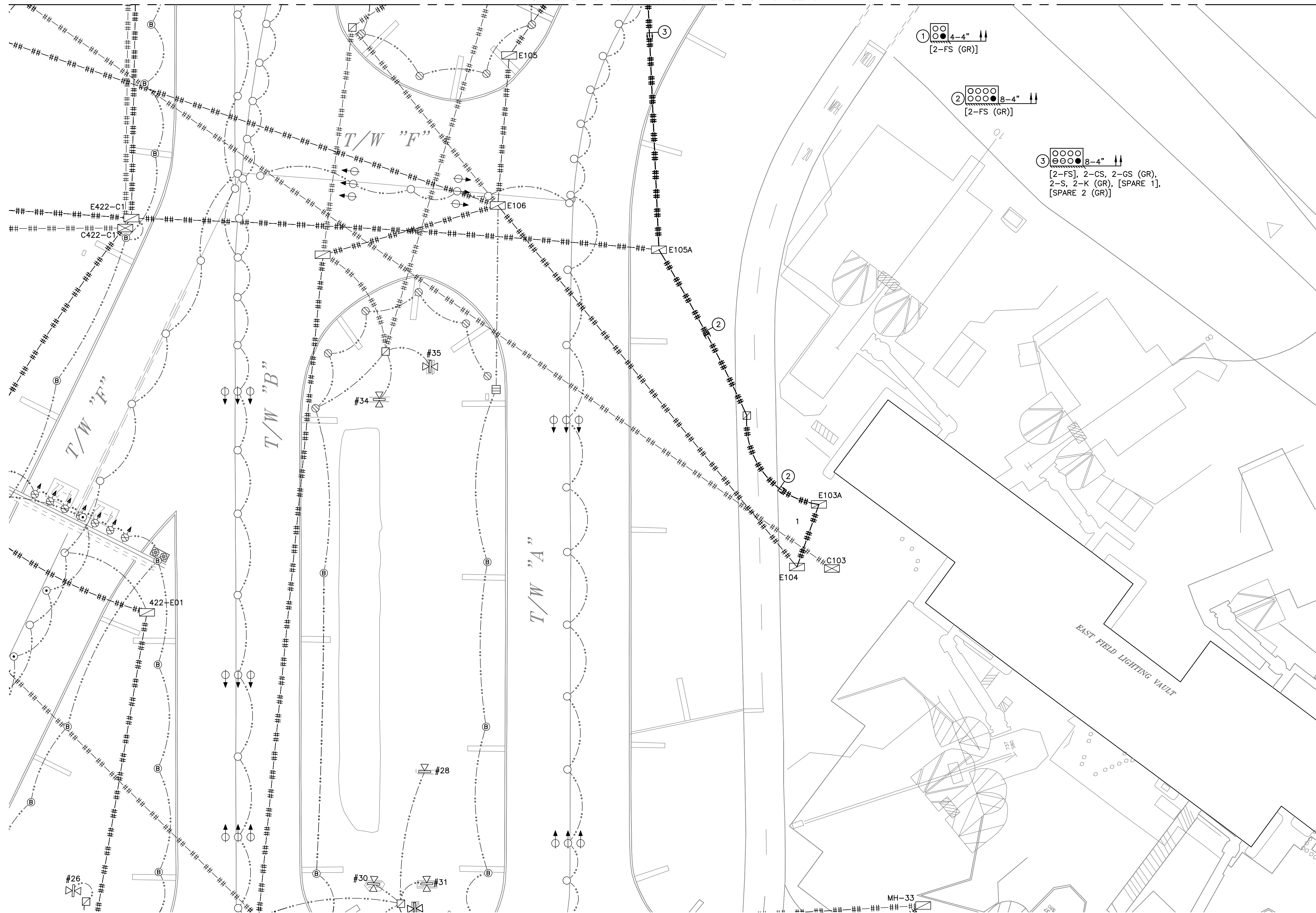


FOR CONTINUATION SEE DWG NO. 202 MATCH LINE A-A FOR CONTINUATION SEE DWG NO. E203



FOR CONTINUATION SEE DWG NO. E207 MATCH LINE C-C FOR CONTINUATION SEE DWG NO. E208

MATCH LINE FOR CONTINUATION SEE DWG NO. E213



**THE PORT AUTHORITY
OF NY & NJ**

CHIEF ELECTRICAL ENGINEER

30% SUBMISSION
07/15/2013

No.	Date	Revision	Approved

ENGINEERING DEPARTMENT

LAGUARDIA
AIRPORT

ELECTRICAL

Title
REPLACEMENT OF THE EAST FIELD
LIGHTING VAULT

INSTALLATION PLAN
-1-

This drawing subject to conditions in contract. All inventions, ideas, designs and methods herein are reserved to Port Authority and may not be used without its written consent. All recipients of Contract documents, including bidders and those who do not bid and their prospective subcontractors and suppliers who may receive all or a part of the Contract documents or copies thereof, shall make every effort to ensure the secure and appropriate disposal of the Contract documents to prevent further disclosure of the information contained in the documents. Secure and appropriate disposal includes methods of document destruction such as shredding or arrangements with refuse handlers that ensure that third persons will not have access to the documents' contents either before, during, or after disposal. Documents may also be returned for disposal purposes to the Contract Desk on the 3rd Floor, 3 Gateway Center, Newark, NJ 07102 or the office of the Director of Procurement, Two Montgomery Street, 3rd Floor, Jersey City, NJ 07302. It is a violation of law for any person to alter a document in any way, unless acting under the direction of a licensed professional engineer or registered architect. If this document, bearing the seal of an engineer/architect is altered, the altering engineer/architect shall affix to the document their seal and the notation "altered by" followed by their signature and the date of such alteration, and a specific description of the alteration.

M. TATARA M. TATARA E. FAYMAN
Designed by Drawn by Checked by

Date 07/15/2013

WORK ORDER No. # X (TBD)
Contract
Number LGA - 124.223

Drawing
Number E212

PID# 12801327

30% SUBMISSION
07/15/2013

No. Date Revision Approved

ENGINEERING DEPARTMENT

LAGUARDIA
AIRPORT

ELECTRICAL

Title
REPLACEMENT OF THE EAST FIELD
LIGHTING VAULT

INSTALLATION PLAN
-2-

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M. TATARA M. TATARA E. FAYFMAN
Designed by Drawn by Checked by

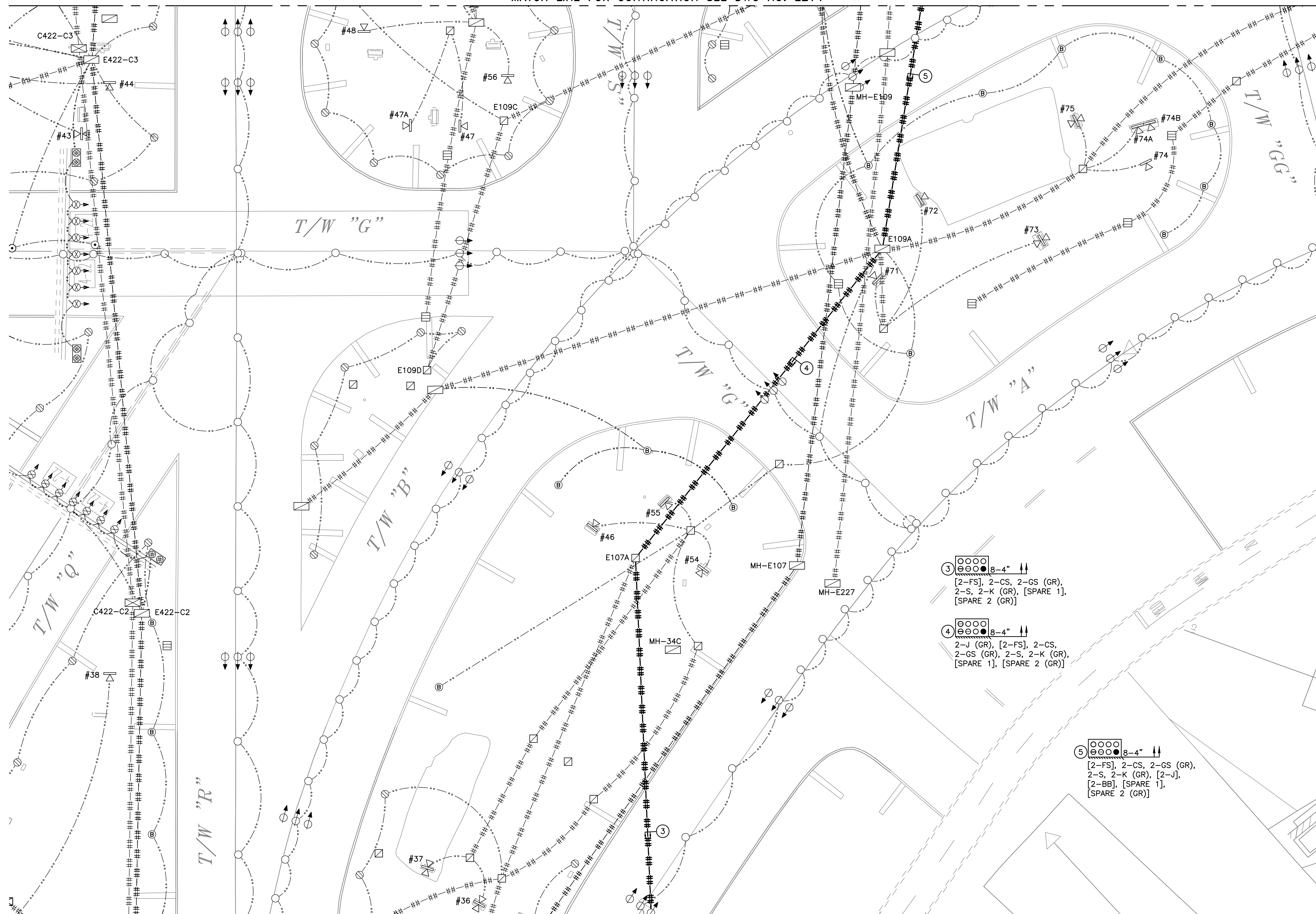
Date 07/15/2013

WORK ORDER No. # X (TBD)
Contract Number LGA - 124.223

Drawing Number E213

PID# 12801327

MATCH LINE FOR CONTINUATION SEE DWG NO. E214



MATCH LINE FOR CONTINUATION SEE DWG NO. E212

30% SUBMISSION
07/15/2013

No.	Date	Revision	Approved

ENGINEERING DEPARTMENT			
LAGUARDIA AIRPORT			

ELECTRICAL
Title
**REPLACEMENT OF THE EAST FIELD
LIGHTING VAULT**

**INSTALLATION PLAN
-3-**

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M. TATARA **M. TATARA** **E. FAYFMAN**
Designed by Drawn by Checked by

Date 07/15/2013
WORK ORDER No. # X (TBD)
Contract
Number **LGA - 124.223**

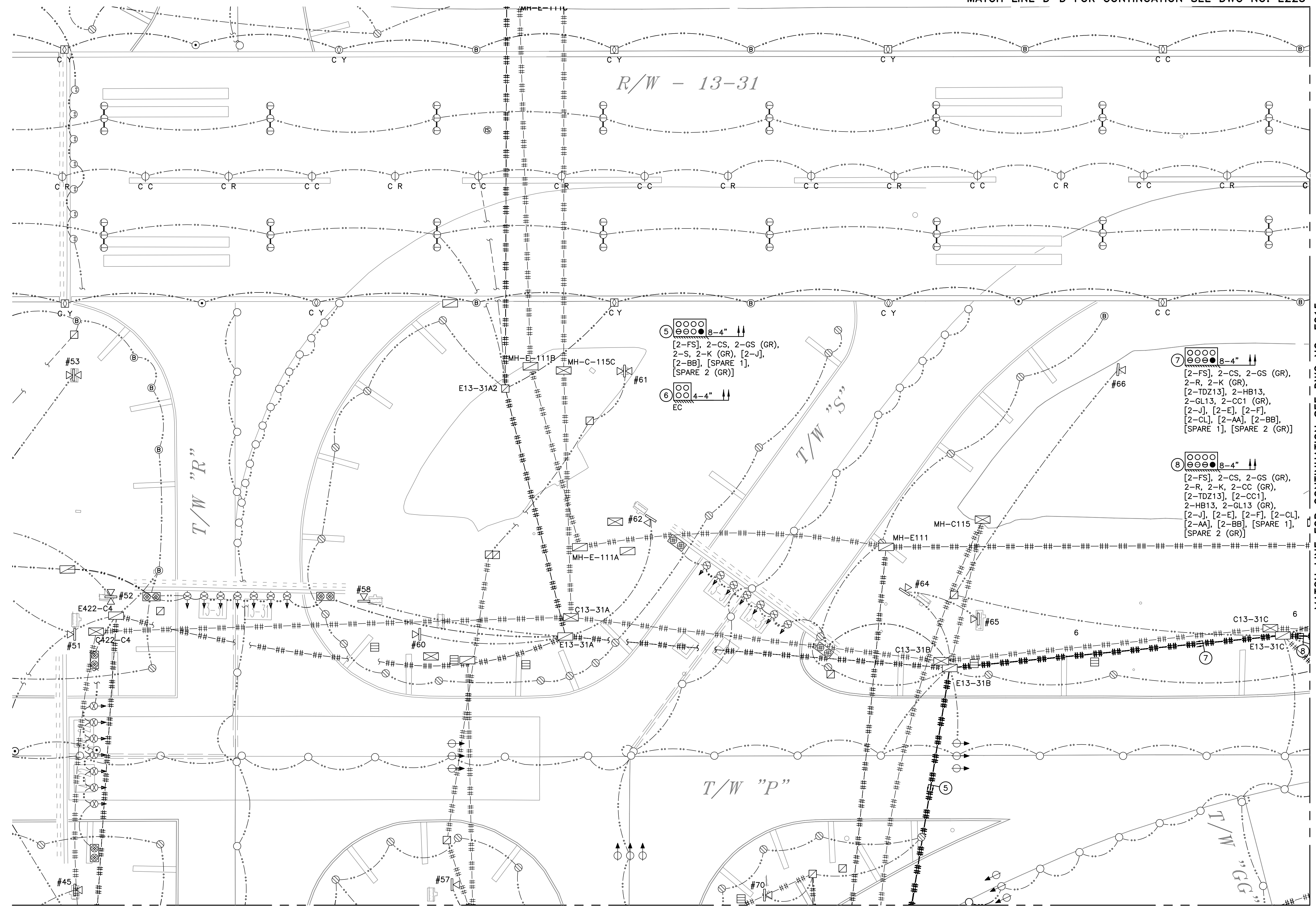
Drawing
Number **E214**
PID# 12801327

MATCH LINE D-D FOR CONTINUATION SEE DWG NO. E223

R/W - 13-31

MATCH LINE FOR CONTINUATION SEE DWG NO. E215

MATCH LINE FOR CONTINUATION SEE DWG NO. E213



CHIEF ELECTRICAL ENGINEER

30% SUBMISSION
07/15/2013

No.	Date	Revision	Approved

ENGINEERING DEPARTMENT

LAGUARDIA
AIRPORT

ELECTRICAL

Title
REPLACEMENT OF THE EAST FIELD
LIGHTING VAULT

INSTALLATION PLAN
-4-

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M. TATAR M. TATAR E. FAYMAN
Designed by Drawn by Checked by

Date 07/15/2013

WORK ORDER No. # X (TBD)
Contract Number LGA - 124.223

Drawing Number E215
PID# 12801327

MATCH LINE D-D FOR CONTINUATION SEE DWG NO. E223

R/W - 13-31

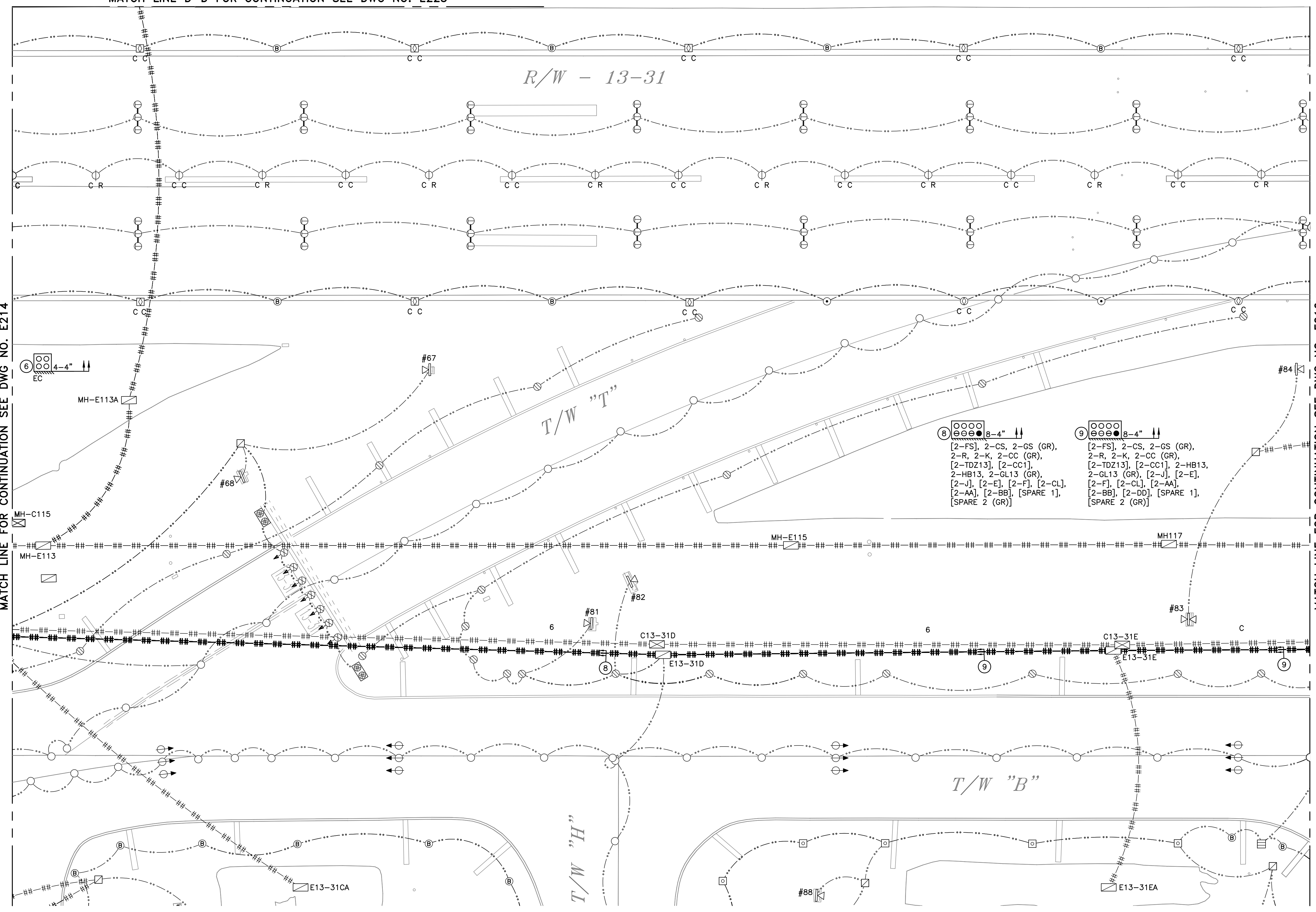
T/W "T"

T/W "B"

T/W "H"

MATCH LINE FOR CONTINUATION SEE DWG NO. E214

MATCH LINE FOR CONTINUATION SEE DWG NO. E216



8 8-4" $\downarrow\downarrow$
[2-FS], 2-CS, 2-GS (GR),
2-R, 2-K, 2-CC (GR),
[2-TDZ13], [2-CC1],
2-HB13, 2-GL13 (GR),
[2-J], [2-E], [2-F], [2-CL],
[2-AA], [2-BB], [SPARE 1],
[SPARE 2 (GR)]

9 8-4" $\downarrow\downarrow$
[2-FS], 2-CS, 2-GS (GR),
2-R, 2-K, 2-CC (GR),
[2-TDZ13], [2-CC1], 2-HB13,
2-GL13 (GR), [2-J], [2-E],
[2-F], [2-CL], [2-AA],
[2-BB], [2-DD], [SPARE 1],
[SPARE 2 (GR)]

6 4-4" $\downarrow\downarrow$
FC

MH-E113A

#67

#84

MH-C115

MH-E113

#68

MH-E115

MH117

#83

#81

#82

6

6

C

C13-31D

E13-31D

C13-31E

E13-31E

9

E13-31CA

E13-31EA

#88

CHIEF ELECTRICAL ENGINEER

30% SUBMISSION
07/15/2013

No.	Date	Revision	Approved

ENGINEERING DEPARTMENT

LAGUARDIA
AIRPORT

ELECTRICAL

Title
REPLACEMENT OF THE EAST FIELD
LIGHTING VAULT

INSTALLATION PLAN
-5-

This drawing subject to conditions in contract. All inventions, ideas, designs and methods herein are reserved to Port Authority and may not be used without its written consent. All recipients of Contract documents, including bidders and those who do not bid and their prospective subcontractors and suppliers who may receive all or a part of the Contract documents or copies thereof, shall make every effort to ensure the secure and appropriate disposal of the Contract documents to prevent further disclosure of the information contained in the documents. Secure and appropriate disposal includes methods of document destruction such as shredding or arrangements with refuse handlers that ensure that third persons will not have access to the documents' contents either before, during, or after disposal. Documents may also be returned for disposal purposes to the Contract Desk on the 3rd Floor, 3 Gateway Center, Newark, NJ 07102 or the office of the Director of Procurement, Two Montgomery Street, 3rd Floor, Jersey City, NJ 07302. It is a violation of law for any person to alter a document in any way, unless acting under the direction of a licensed professional engineer or registered architect. If this document, bearing the seal of an engineer/architect is altered, the altering engineer/architect shall affix to the document their seal and the notation "altered by" followed by their signature and the date of such alteration, and a specific description of the alteration.

M. TATARA M. TATARA E. FAYFMAN
Designed by Drawn by Checked by

Date 07/15/2013

WORK ORDER No. # X (TBD)
Contract
Number LGA - 124.223

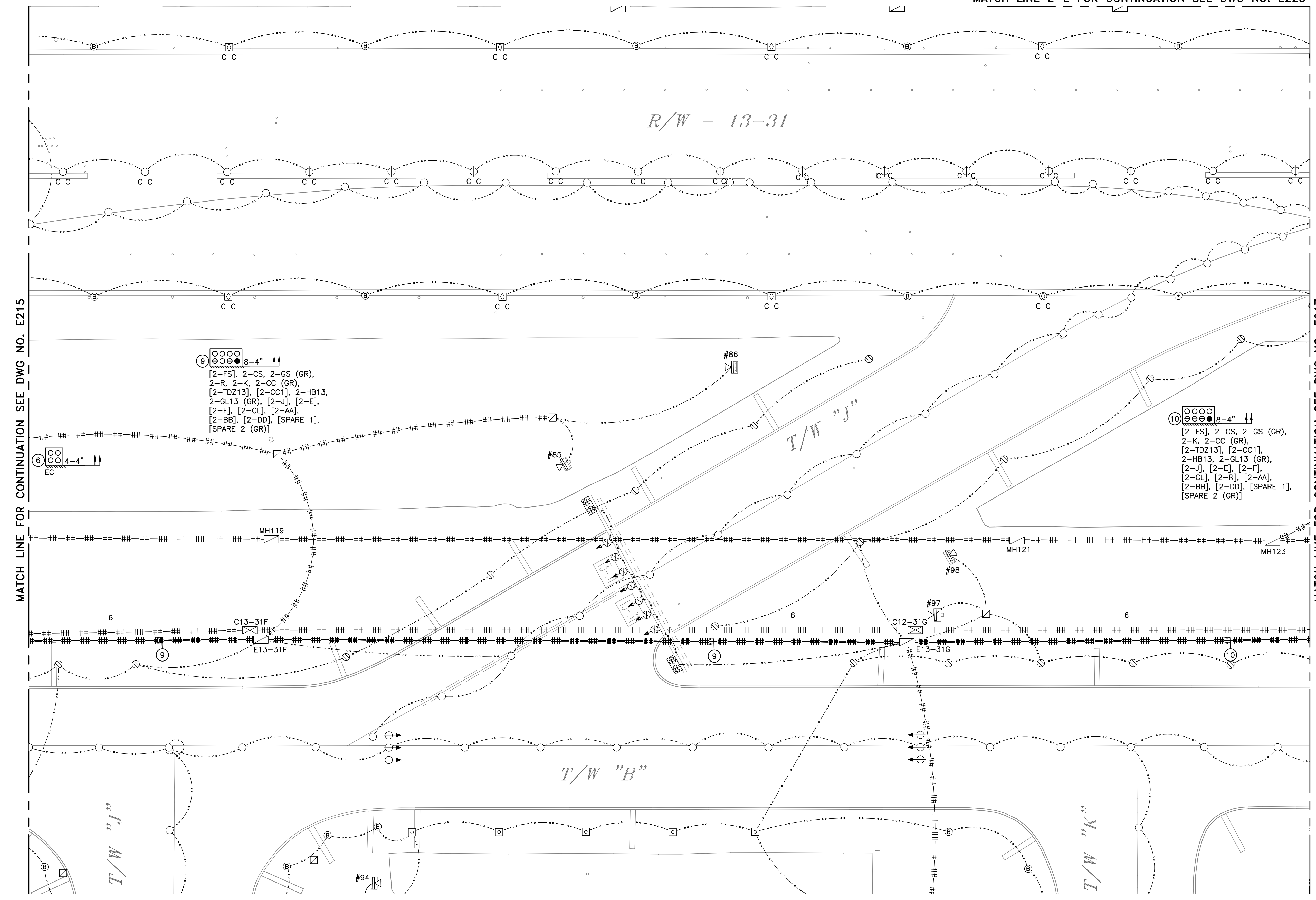
Drawing
Number **E216**
PID# 12801327

MATCH LINE E-E FOR CONTINUATION SEE DWG NO. E223

R/W - 13-31

MATCH LINE FOR CONTINUATION SEE DWG NO. E215

MATCH LINE FOR CONTINUATION SEE DWG NO. E217



CHIEF ELECTRICAL ENGINEER

30% SUBMISSION
07/15/2013

No.	Date	Revision	Approved

ENGINEERING DEPARTMENT

**LAGUARDIA
AIRPORT**

ELECTRICAL

Title
REPLACEMENT OF THE EAST FIELD
LIGHTING VAULT

**INSTALLATION PLAN
-6-**

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M. TATARA M. TATARA E. FAYMAN
Designed by Drawn by Checked by

Date 07/15/2013

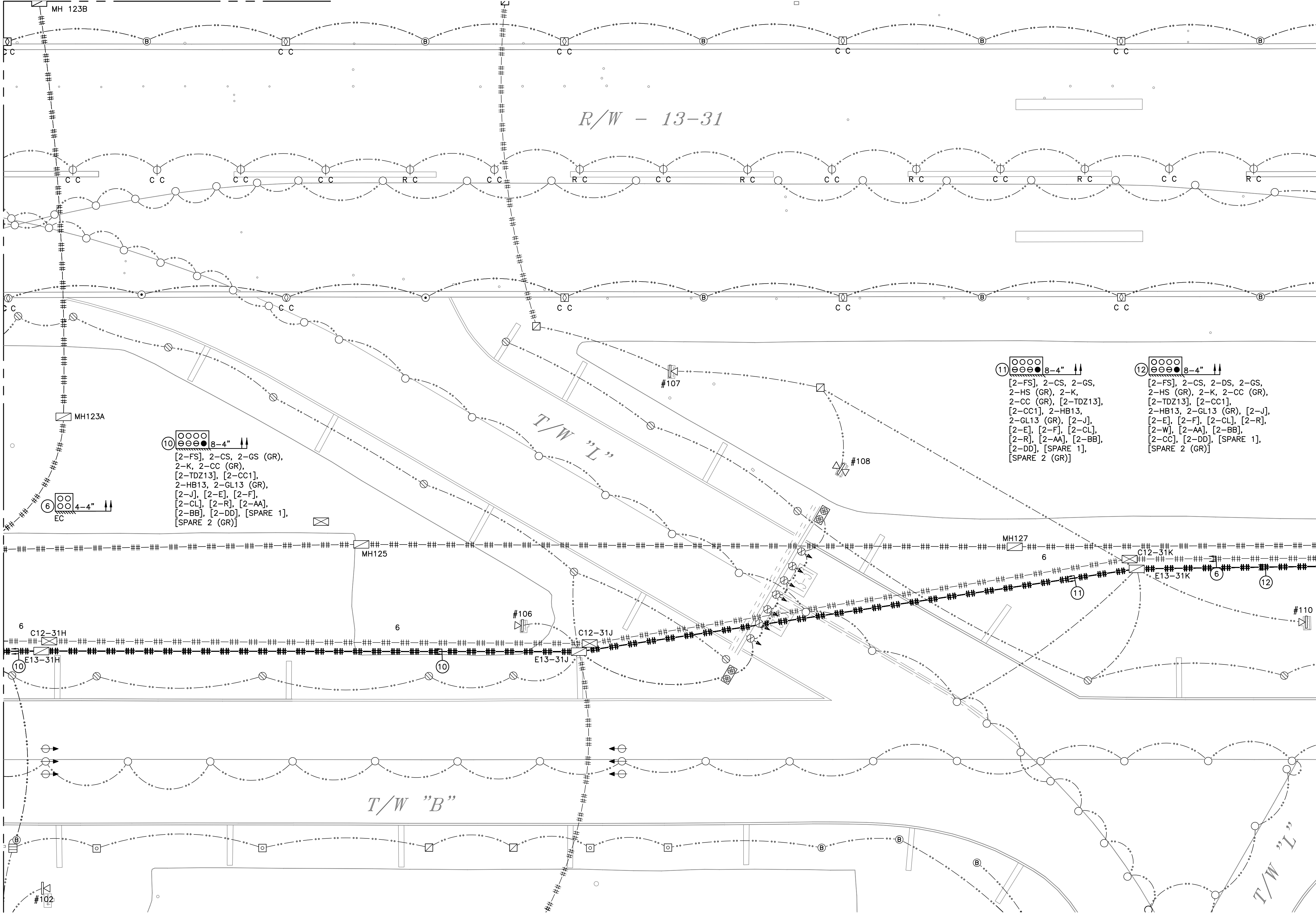
WORK ORDER No. # X (TBD)
Contract
Number **LGA - 124.223**

Drawing
Number **E217**
PID# 12801327

MATCH LINE E-E FOR CONTINUATION SEE DWG NO. E223

MATCH LINE FOR CONTINUATION SEE DWG NO. E216

MATCH LINE FOR CONTINUATION SEE DWG NO. E218



⑩ 8-4" ↑↑
[2-FS], 2-CS, 2-GS (GR),
2-K, 2-CC (GR),
[2-TDZ13], [2-CC1],
2-HB13, 2-GL13 (GR),
[2-J], [2-E], [2-F],
[2-CL], [2-R], [2-AA],
[2-BB], [2-DD], [SPARE 1],
[SPARE 2 (GR)]

⑪ 8-4" ↑↑
[2-FS], 2-CS, 2-GS,
2-HS (GR), 2-K,
2-CC (GR), [2-TDZ13],
[2-CC1], 2-HB13,
2-GL13 (GR), [2-J],
[2-E], [2-F], [2-CL],
[2-R], [2-AA], [2-BB],
[2-DD], [SPARE 1],
[SPARE 2 (GR)]

⑫ 8-4" ↑↑
[2-FS], 2-CS, 2-DS, 2-GS,
2-HS (GR), 2-K, 2-CC (GR),
[2-TDZ13], [2-CC1],
2-HB13, 2-GL13 (GR), [2-J],
[2-E], [2-F], [2-CL], [2-R],
[2-W], [2-AA], [2-BB],
[2-CC], [2-DD], [SPARE 1],
[SPARE 2 (GR)]

T/W "B"

T/W "L"

T/W "L"

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07/15/2013

No.	Date	Revision	Approved

ENGINEERING DEPARTMENT			
LAGUARDIA AIRPORT			

ELECTRICAL

Title
**REPLACEMENT OF THE EAST FIELD
LIGHTING VAULT**

**INSTALLATION PLAN
-7-**

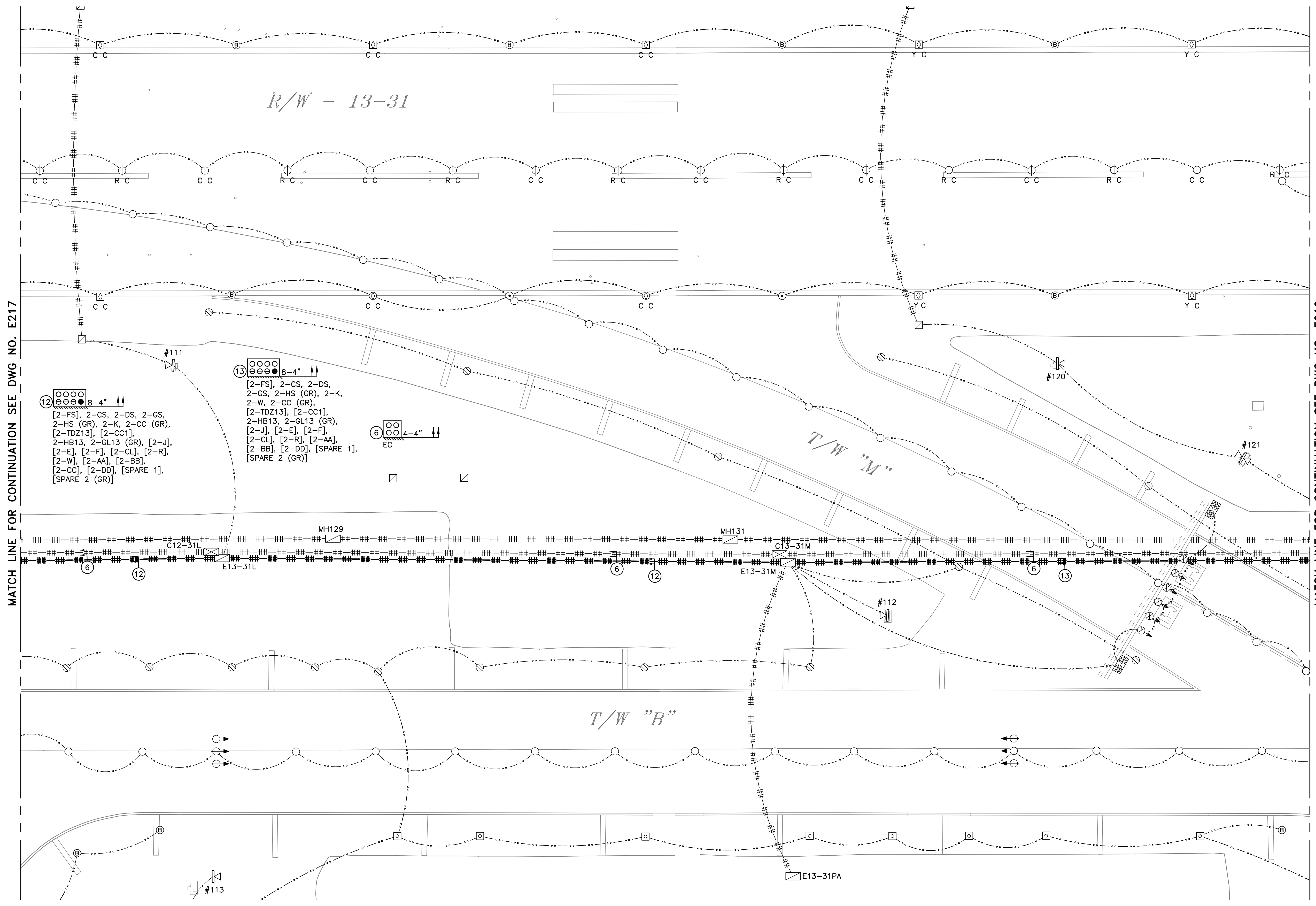
This drawing subject to conditions in contract. All inventions, ideas, designs and methods herein are reserved to Port Authority and may not be used without its written consent. All recipients of Contract documents, including bidders and those who do not bid and their prospective subcontractors and suppliers who may receive all or a part of the Contract documents or copies thereof, shall make every effort to ensure the secure and appropriate disposal of the Contract documents to prevent further disclosure of the information contained in the documents. Secure and appropriate disposal includes methods of document destruction such as shredding or arrangements with refuse handlers that ensure that third persons will not have access to the documents' contents either before, during, or after disposal. Documents may also be returned for disposal purposes to the Contract Desk on the 3rd Floor, 3 Gateway Center, Newark, NJ 07102 or the office of the Director of Procurement, Two Montgomery Street, 3rd Floor, Jersey City, NJ 07302. It is a violation of law for any person to alter a document in any way, unless acting under the direction of a licensed professional engineer or registered architect. If this document, bearing the seal of an engineer/architect is altered, the altering engineer/architect shall affix to the document their seal and the notation "altered by" followed by their signature and the date of such alteration, and a specific description of the alteration.

M. TATAR **M. TATAR** **E. FAYFMAN**
Designed by Drawn by Checked by

Date 07/15/2013

WORK ORDER No. # X (TBD)
Contract Number **LGA - 124.223**

Drawing Number **E218**
PID# 12801327



MATCH LINE FOR CONTINUATION SEE DWG NO. E217

MATCH LINE FOR CONTINUATION SEE DWG NO. E219

**THE PORT AUTHORITY
OF NY & NJ**

CHIEF ELECTRICAL ENGINEER

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07/15/2013

No.	Date	Revision	Approved

ENGINEERING DEPARTMENT			
LAGUARDIA AIRPORT			

ELECTRICAL

Title
**REPLACEMENT OF THE EAST FIELD
LIGHTING VAULT**

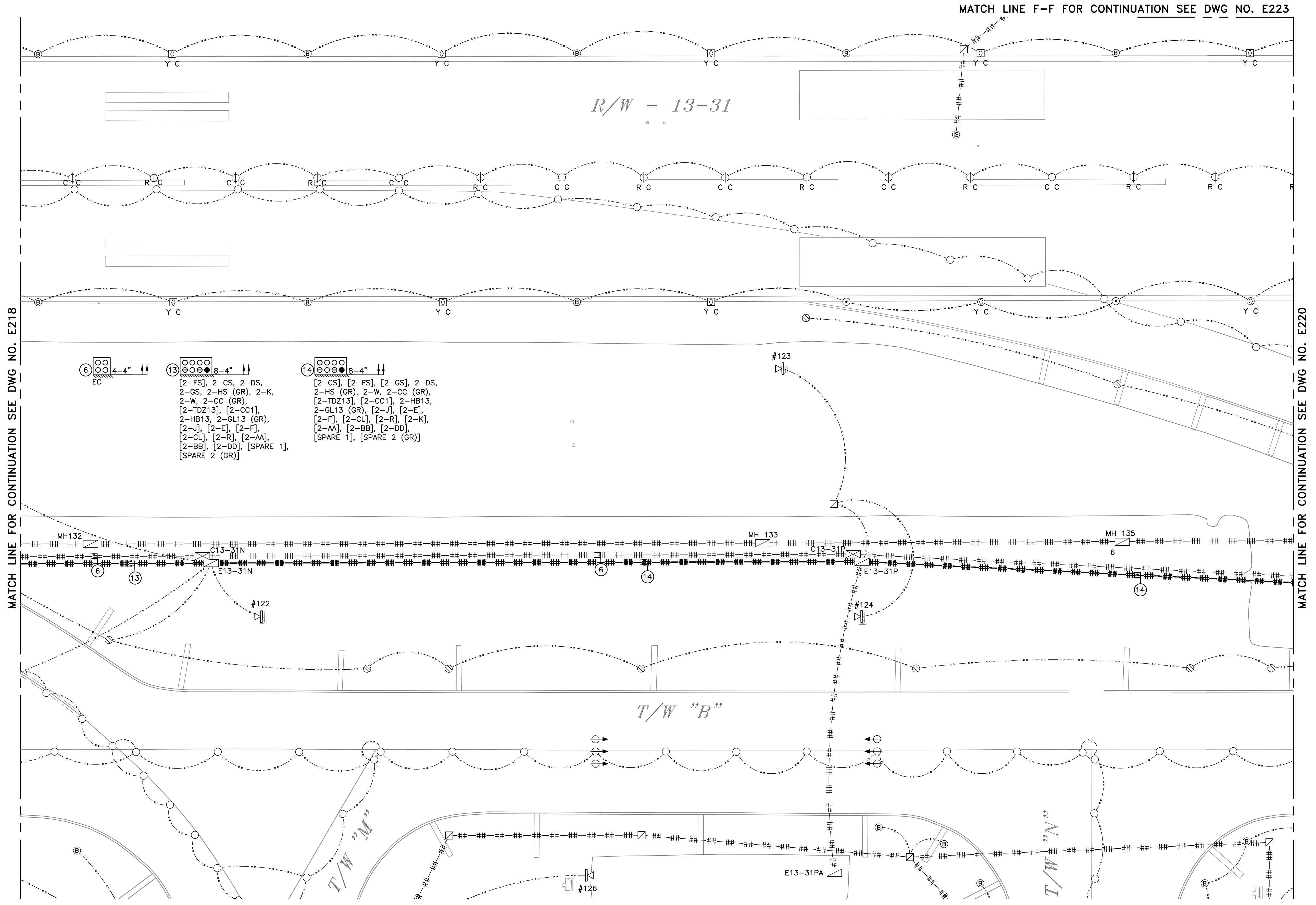
**INSTALLATION PLAN
-8-**

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M. TATARA **M. TATARA** **E. FAYMAN**
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Date 07/15/2013
WORK ORDER No. # X (TBD)
Contract Number **LGA - 124.223**

Drawing Number **E219**
PID# 12801327



CHIEF ELECTRICAL ENGINEER

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 07/15/2013

No.	Date	Revision	Approved

ENGINEERING DEPARTMENT			
LAGUARDIA AIRPORT			

ELECTRICAL

Title
REPLACEMENT OF THE EAST FIELD LIGHTING VAULT

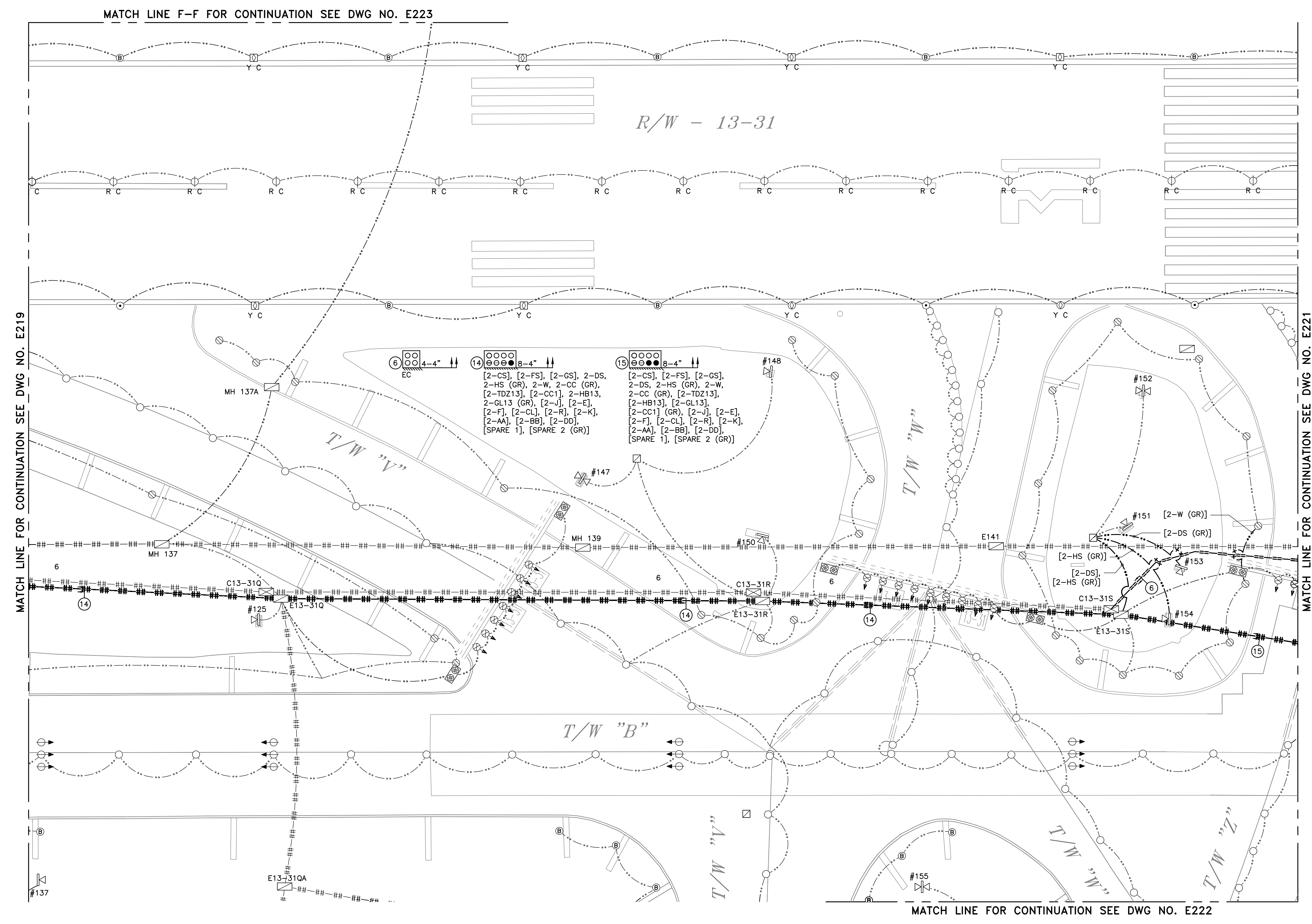
INSTALLATION PLAN
-9-

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M. TATARA **M. TATARA** **E. FAYMAN**
 Designed by Drawn by Checked by

Date 07/15/2013
WORK ORDER No. # X (TBD)
 Contract Number **LGA - 124.223**

Drawing Number **E220**
 PID# 12801327



MATCH LINE F-F FOR CONTINUATION SEE DWG NO. E223

R/W - 13-31

MATCH LINE FOR CONTINUATION SEE DWG NO. E219

MATCH LINE FOR CONTINUATION SEE DWG NO. E221

MATCH LINE FOR CONTINUATION SEE DWG NO. E222

CHIEF ELECTRICAL ENGINEER

30% SUBMISSION
07/15/2013

No.	Date	Revision	Approved

ENGINEERING DEPARTMENT			
LAGUARDIA AIRPORT			

ELECTRICAL

Title
**REPLACEMENT OF THE EAST FIELD
LIGHTING VAULT**

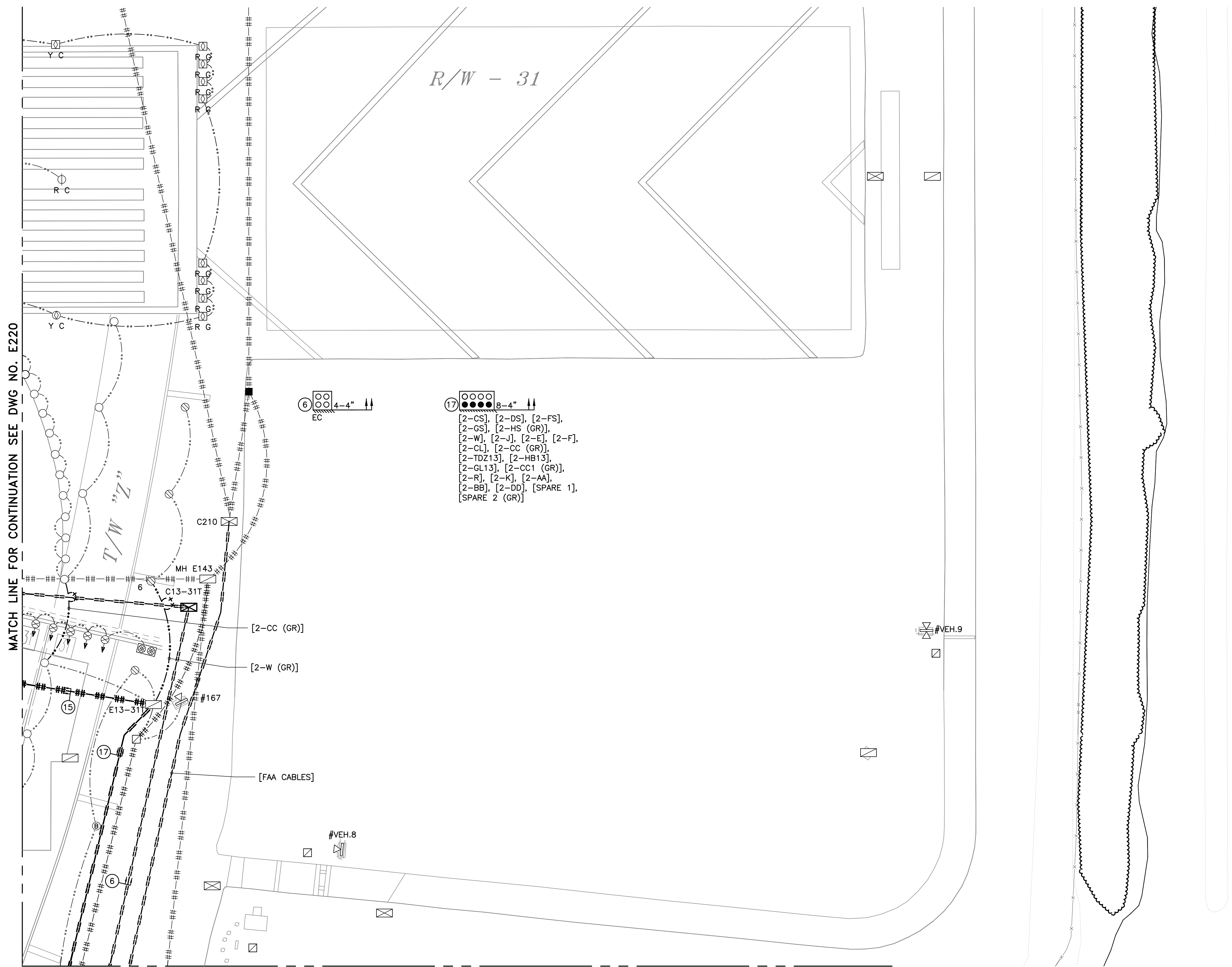
**INSTALLATION PLAN
- 10 -**

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M. TATARA **M. TATARA** **E. FAYMAN**
Designed by Drawn by Checked by

Date 07/15/2013
WORK ORDER No. # X (TBD)
Contract Number **LGA - 124.223**

Drawing Number **E221**
PID# 12801327



MATCH LINE FOR CONTINUATION SEE DWG NO. E220

MATCH LINE FOR CONTINUATION SEE DWG NO. E222

30% SUBMISSION
07/15/2013

No.	Date	Revision	Approved

ENGINEERING DEPARTMENT

LAGUARDIA
AIRPORT

ELECTRICAL

Title
REPLACEMENT OF THE EAST FIELD
LIGHTING VAULT

INSTALLATION PLAN
- 11 -

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M. TATAR M. TATAR E. FAYMAN
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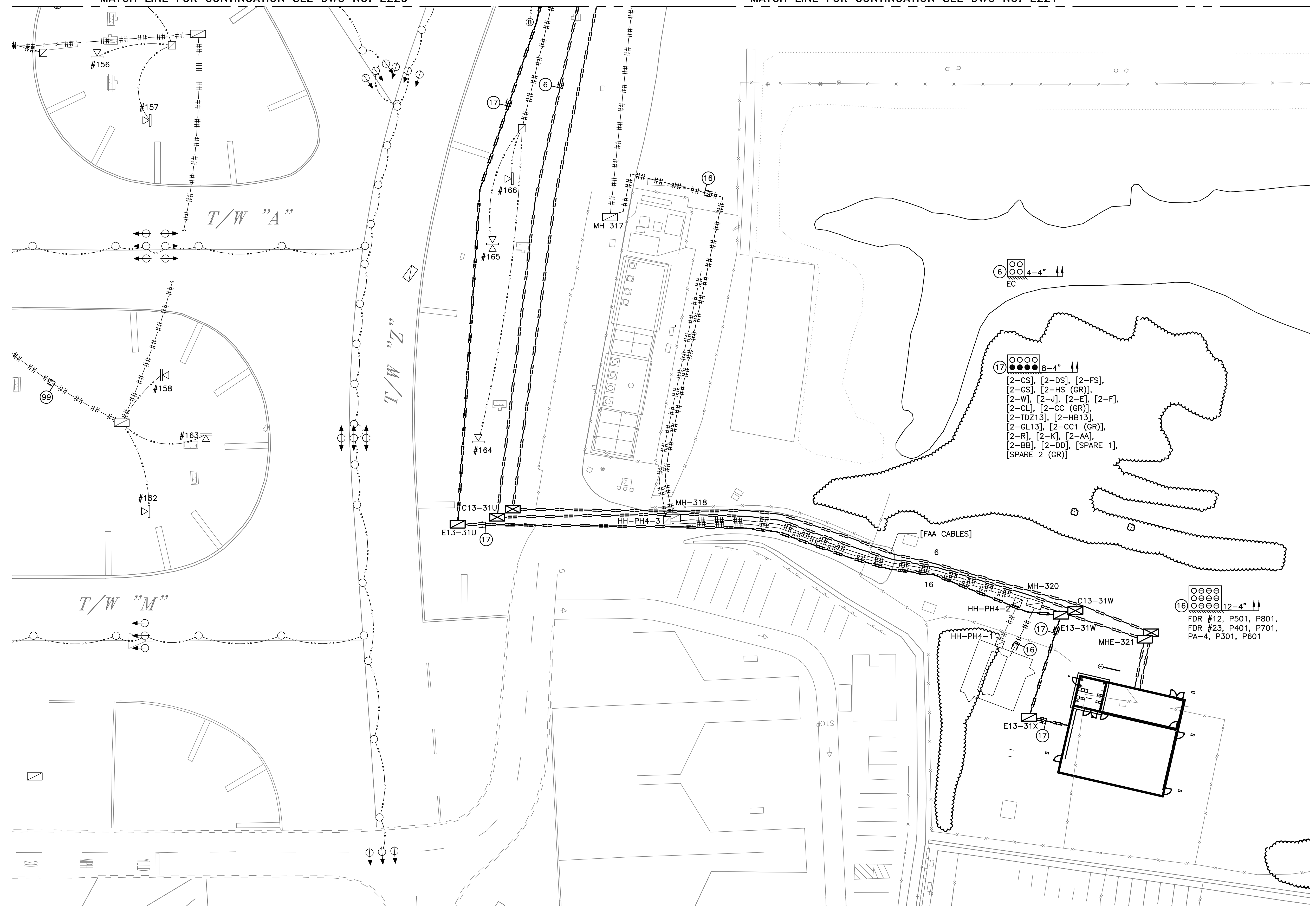
Date 07/15/2013

WORK ORDER No. # X (TBD)
Contract LGA - 124.223
Number

Drawing E222
Number PID# 12801327

MATCH LINE FOR CONTINUATION SEE DWG NO. E220

MATCH LINE FOR CONTINUATION SEE DWG NO. E221



**THE PORT AUTHORITY
OF NY & NJ**

CHIEF ELECTRICAL ENGINEER

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07/15/2013

No.	Date	Revision	Approved

ENGINEERING DEPARTMENT			
LAGUARDIA AIRPORT			

ELECTRICAL

Title
**REPLACEMENT OF THE EAST FIELD
LIGHTING VAULT**

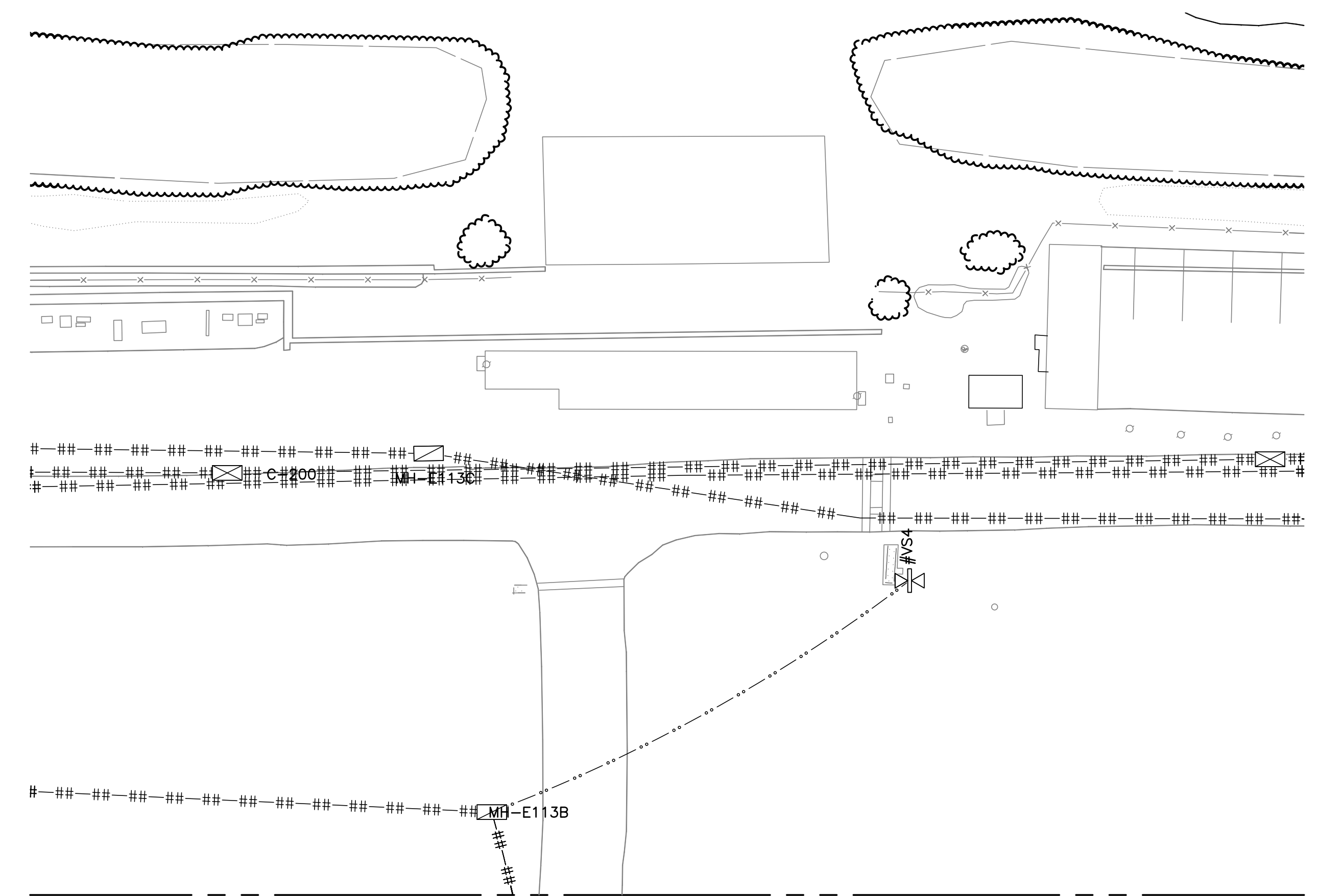
**PARTIAL PLAN
-1-**

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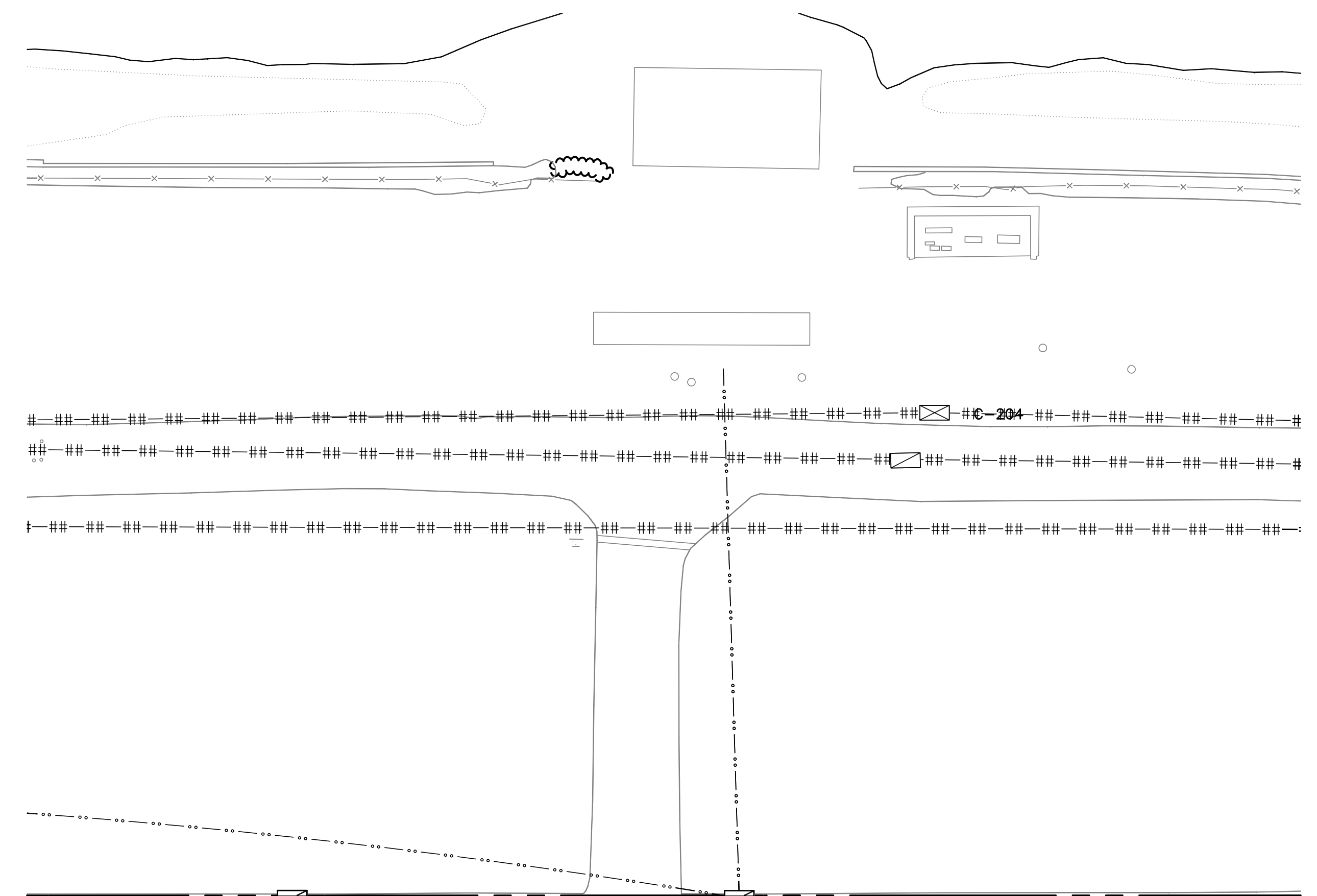
M. TATARA **M. TATARA** **E. FAYMAN**
Designed by Drawn by Checked by

Date 07/15/2013
WORK ORDER No. # X (TBD)
Contract Number **LGA - 124.223**

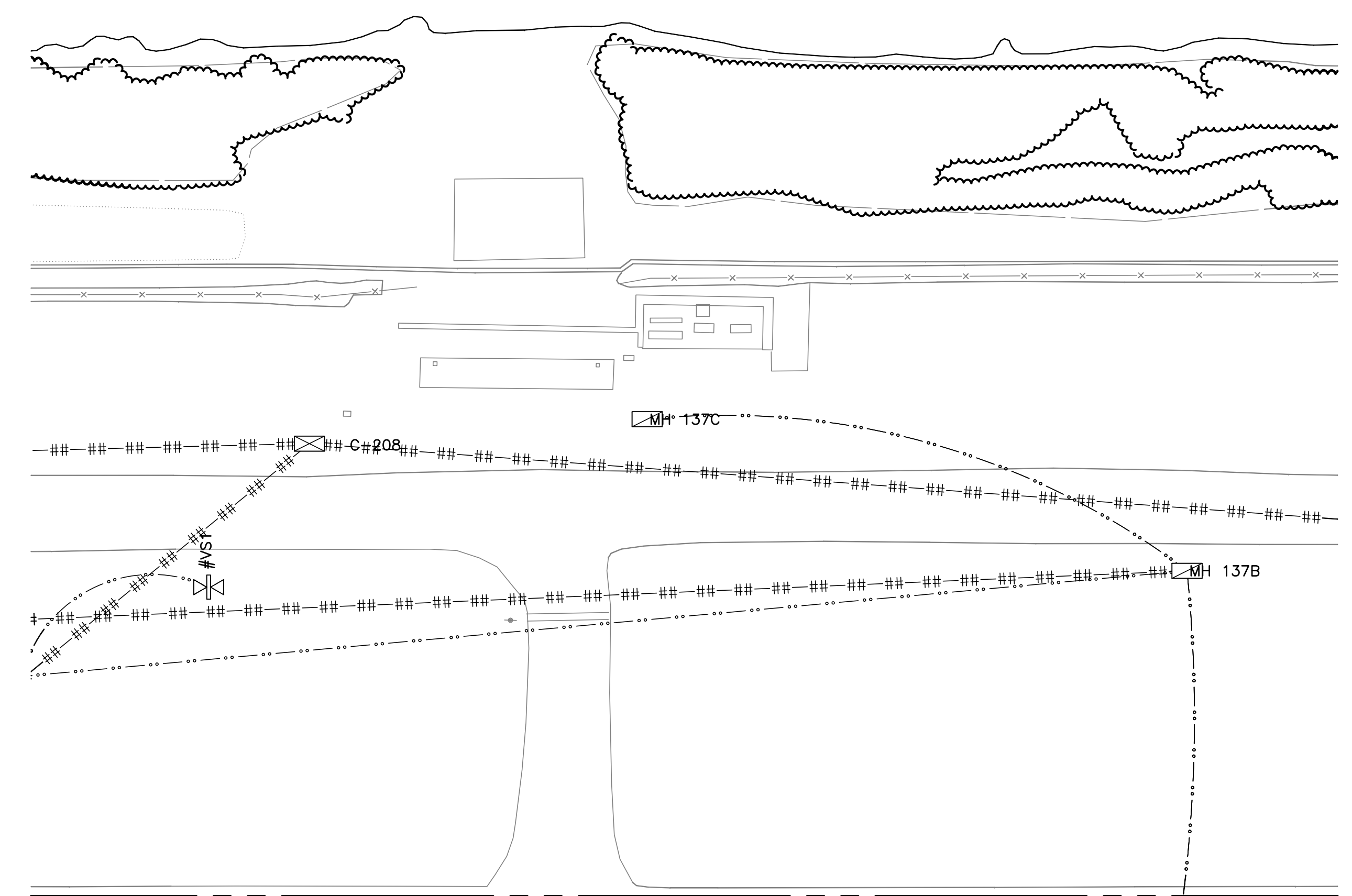
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PID# 12801327



FOR CONTINUATION SEE DWG NO. E214 MATCH LINE D-D FOR CONTINUATION SEE DWG NO. E215



FOR CONTINUATION SEE DWG NO. E216 MATCH LINE E-E FOR CONTINUATION SEE DWG NO. E217



FOR CONTINUATION SEE DWG NO. E219 MATCH LINE F-F FOR CONTINUATION SEE DWG NO. E220

CHIEF ELECTRICAL ENGINEER

30% SUBMISSION
 07/15/2013

No.	Date	Revision	Approved

ENGINEERING DEPARTMENT			
LAGUARDIA AIRPORT			

ELECTRICAL

Title
REPLACEMENT OF THE EAST FIELD LIGHTING VAULT

MAIN ELECTRICAL EQUIPMENT INSTALLATION PLAN

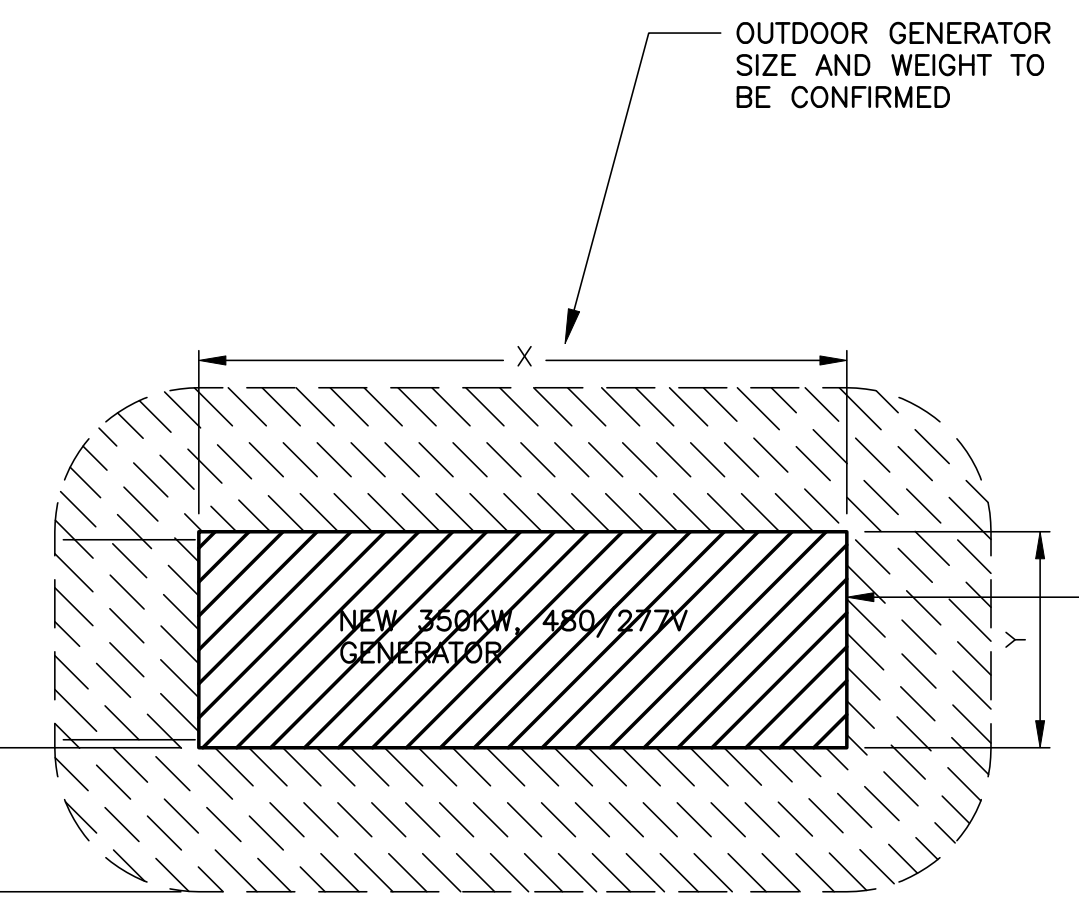
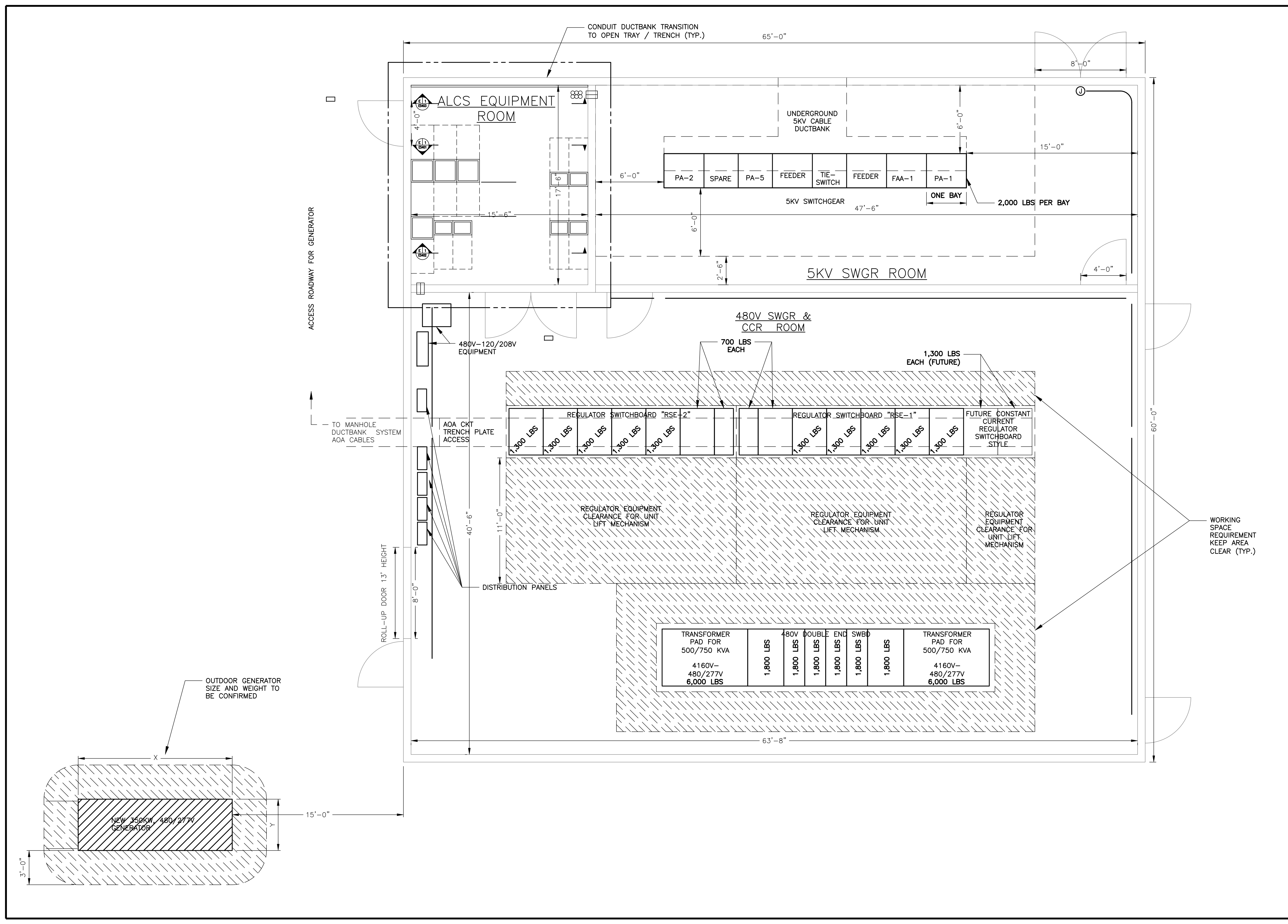
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G. GADOMSKI G. GADOMSKI E. FAYFMAN
 Designed by Drawn by Checked by

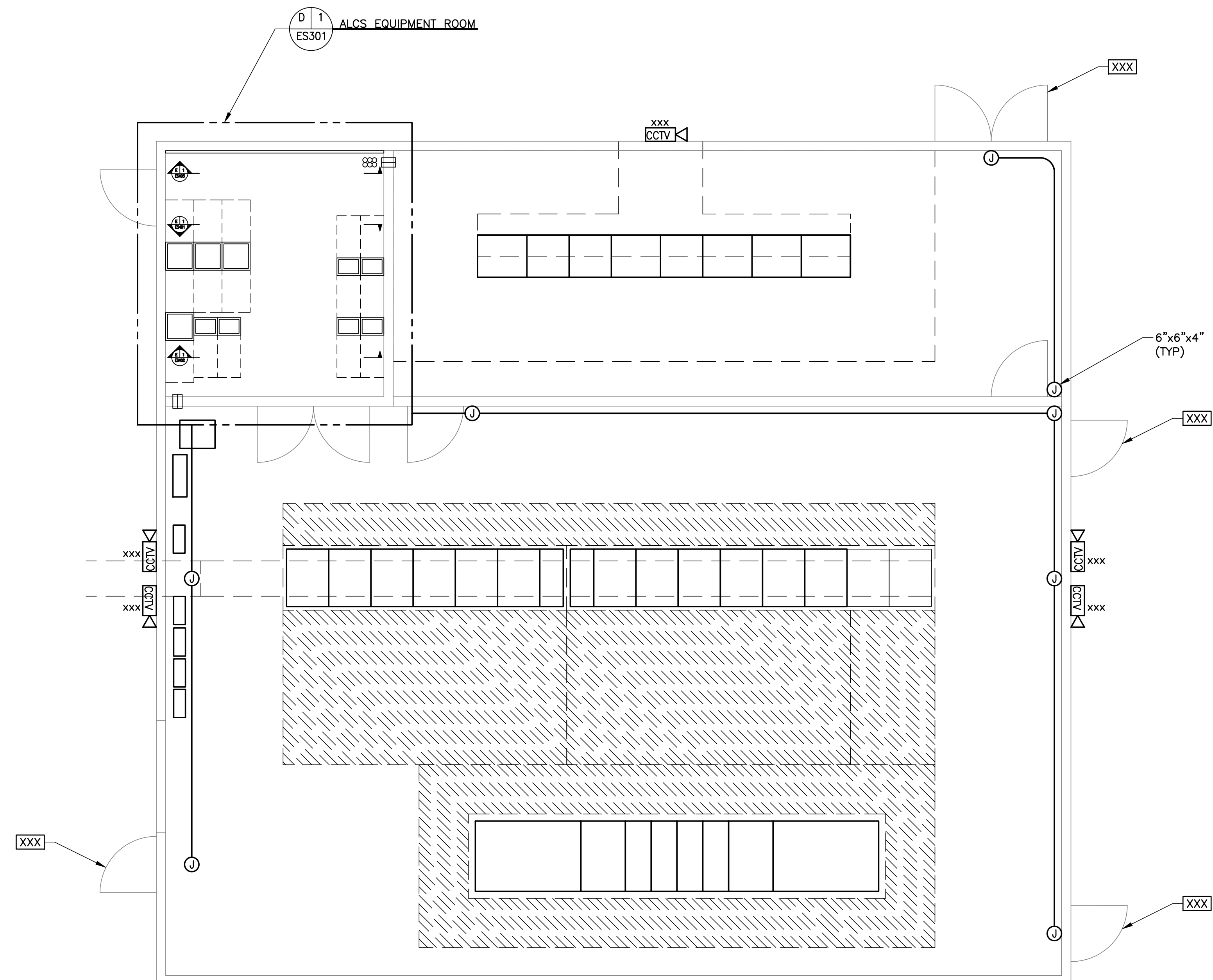
Date **07/15/2013**

WORK ORDER No. # X (TBD)
 Contract Number **LGA - 124.223**

Drawing Number **E251**
 PID# 12801327



NOTES:
 1. FOR LEGEND AND ABBREVIATIONS, SEE CONTRACT DRAWING ES001.



Sheet of

**THE PORT AUTHORITY
 OF NY & NJ**

CHIEF ELECTRICAL ENGINEER

**30% SUBMISSION
 07/15/2013**

No.	Date	Revision	Approved

ENGINEERING DEPARTMENT

**LAGUARDIA
 AIRPORT**

**ELECTRICAL
 ELECTRONICS**

Title
**REPLACEMENT OF THE EAST FIELD
 LIGHTING VAULT**

**FIELD LIGHTING VAULT -
 ELECTRONICS FLOOR
 PLAN**

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C. REED **C. REED** **M. GAMBÀ**
 Designed by Drawn by Checked by

Date **07/15/2013**

WORK ORDER No. # X (TBD)
 Contract Number **LGA - 124.223**

Drawing Number **ES201**
 PID# 12801327

CHIEF ELECTRICAL ENGINEER

30% SUBMISSION
07/15/2013

No.	Date	Revision	Approved

ENGINEERING DEPARTMENT			
LAGUARDIA AIRPORT			

**ELECTRICAL
ELECTRONICS**

Title
**REPLACEMENT OF THE EAST FIELD
LIGHTING VAULT**

**FIELD LIGHTING VAULT -
ALCS EQUIPMENT ROOM**

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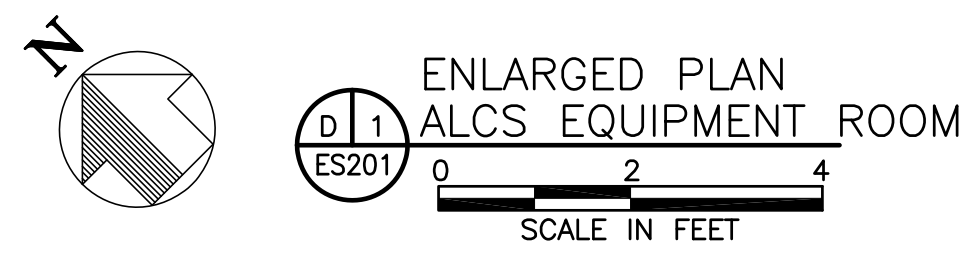
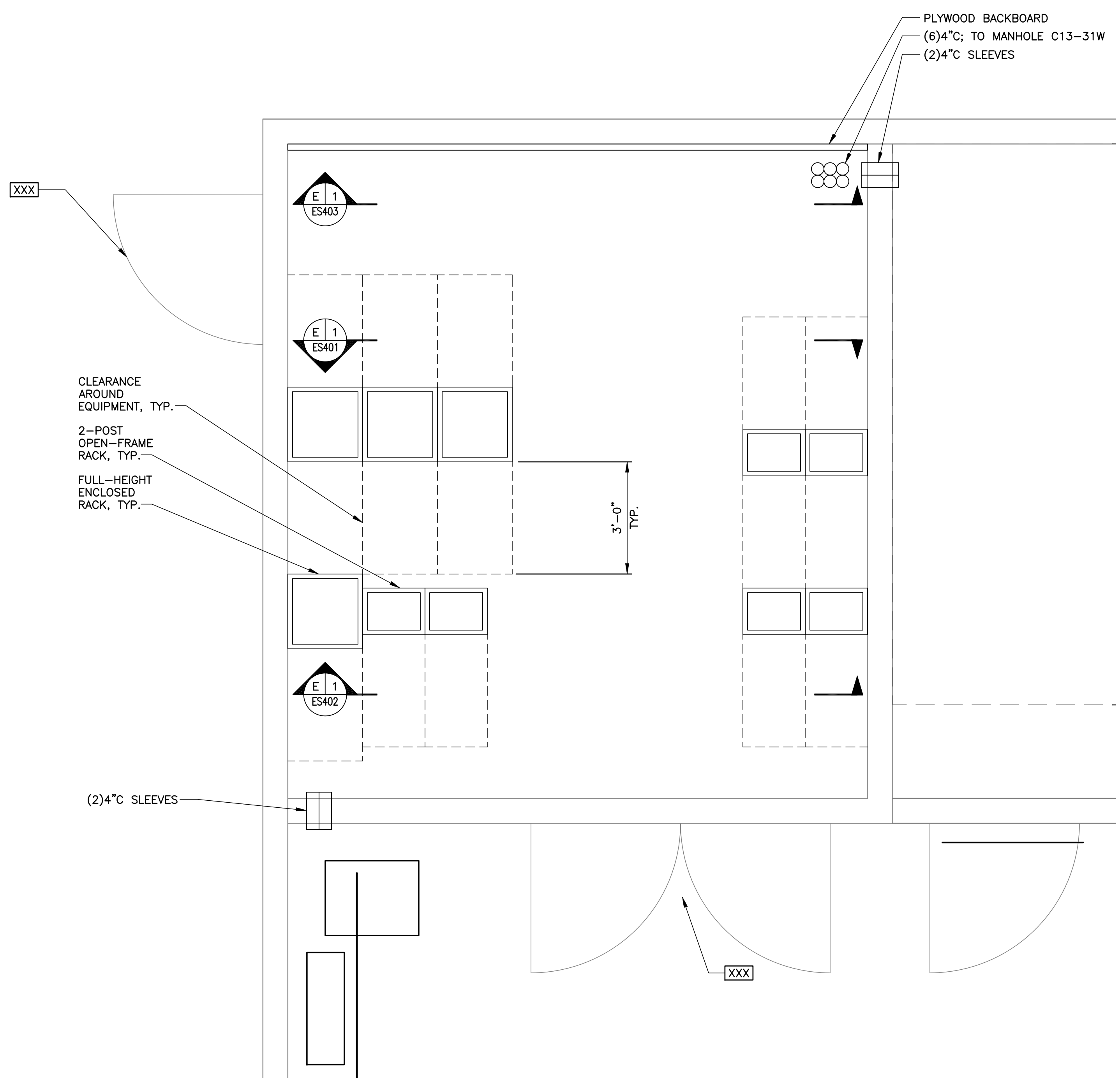
C. REED **C. REED** **M. GAMBIA**
Designed by Drawn by Checked by

Date **07/15/2013**
WORK ORDER No. # X (TBD)
Contract Number **LGA - 124.223**

Drawing Number **ES301**
PID# 12801327

NOTES:

- FOR LEGEND AND ABBREVIATIONS, SEE CONTRACT DRAWING ES001.



ENLARGED PLAN
ALCS EQUIPMENT ROOM

**THE PORT AUTHORITY
OF NY & NJ**

CHIEF ELECTRICAL ENGINEER

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No.	Date	Revision	Approved

ENGINEERING DEPARTMENT

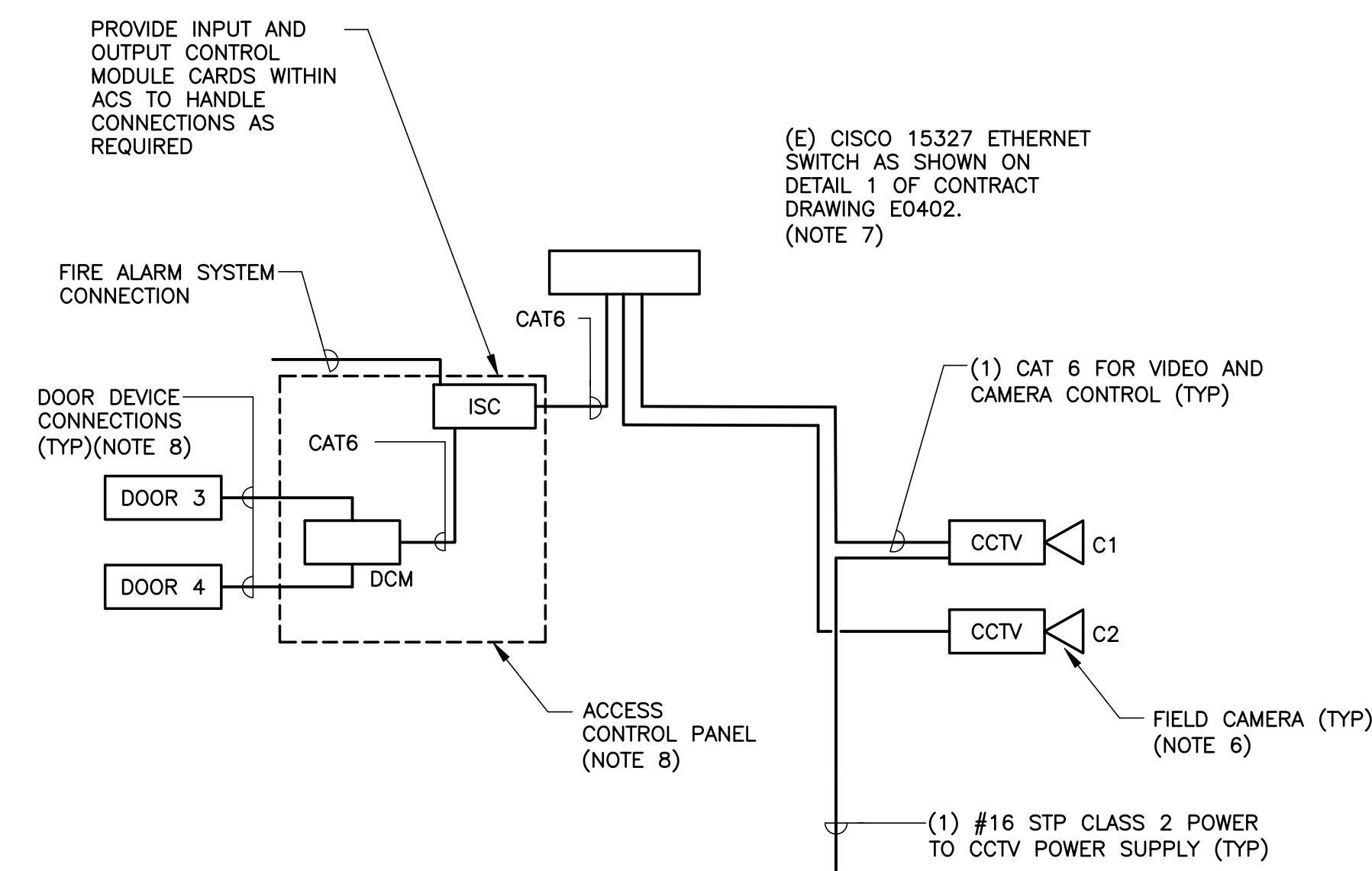
**ELECTRICAL
ELECTRONICS**
Title
**REPLACEMENT OF THE EAST FIELD
LIGHTING VAULT**

**FIBER OPTIC
BLOCK DIAGRAM**

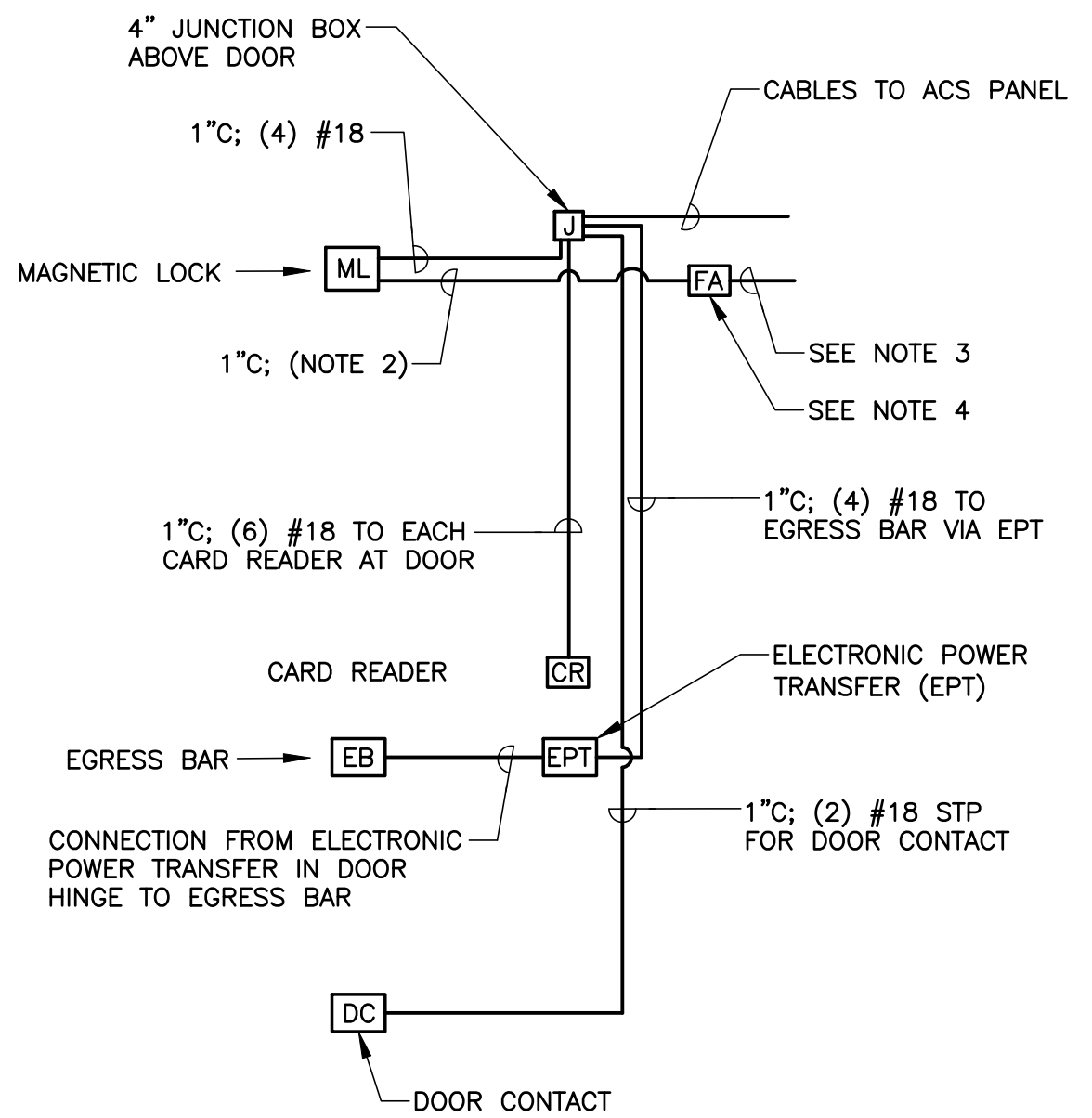
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C. REED	C. REED	M. GAMBA
Designed by	Drawn by	Checked by
Date	07/15/2013	
WORK ORDER No. # X (TBD)		
Contract Number LGA - 124.223		
Drawing Number		ES501
		PID# 12801327

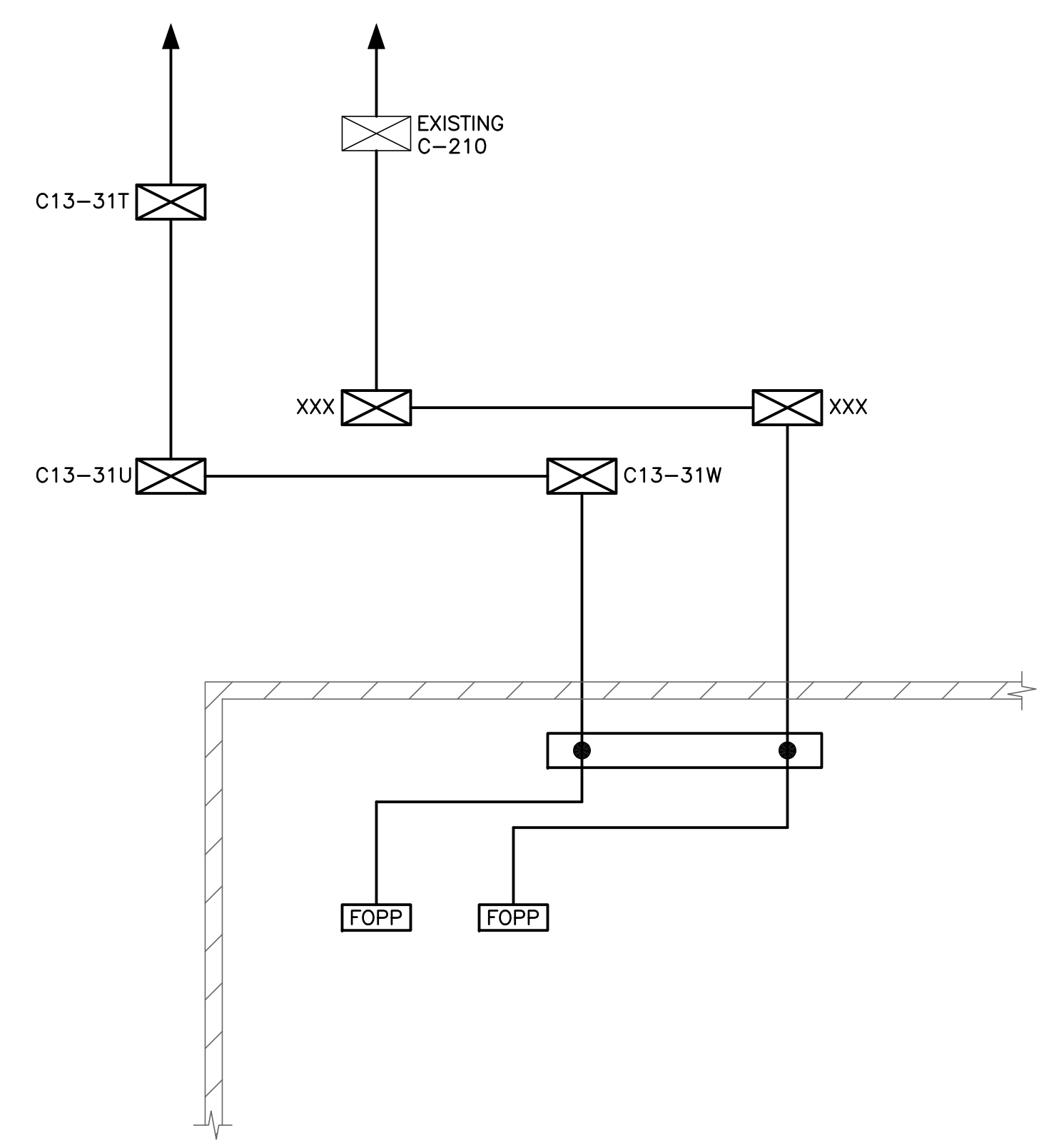
NOTES:
1. FOR LEGEND AND ABBREVIATIONS, SEE CONTRACT DRAWING ES001.



ACS/CCTV BLOCK DIAGRAM
NTS



DOOR DEVICE CONNECTION DIAGRAM
NTS



FIBER OPTIC BLOCK DIAGRAM
NTS